

TOWN OF RICHMOND HILL

COMMITTEE OF THE WHOLE

April 16, 2007
SRPD.07.030

Planning and Development Department
Planning Division

SUBJECT: Hughey West Infill Study - An updated residential infill study for the Bathurst Street Neighbourhood - File No. D18-07003 (SRPD.07.030)

PURPOSE:

To seek endorsement of the Hughey West Infill Study by Committee of the Whole and Council.

RECOMMENDATIONS:

- a) That SRPD.07.030 be received and that Council endorse the Hughey West Infill Study and Urban Design Guidelines prepared by The Planning Partnership, attached as Appendix "A" to SRPD.07.030, as the basis for considering future development and redevelopment in the Bathurst Street neighbourhood;
- b) That the Urban Design Guidelines prepared by The Planning Partnership and contained in Part B of the Hughey West Infill Study, attached as Appendix "A" to SRPD.07.030, be utilized as the urban design control program for future development within the Bathurst Street neighbourhood;
- c) That the Hughey West Infill Study and Urban Design Guidelines augment the existing design guidelines of the Bathurst and Puccini Neighbourhoods Infill Study as the basis for evaluating applications for development and redevelopment within the Bathurst Street neighbourhood, as shown on Schedule "B" to Official Plan Amendment No. 129, attached as Map 1 to SRPD.07.030.

Contact: Bruce Robb, Planner II, extension 2459 and/or
Kelvin Kwan, Acting Director of Planning, extension 2410.

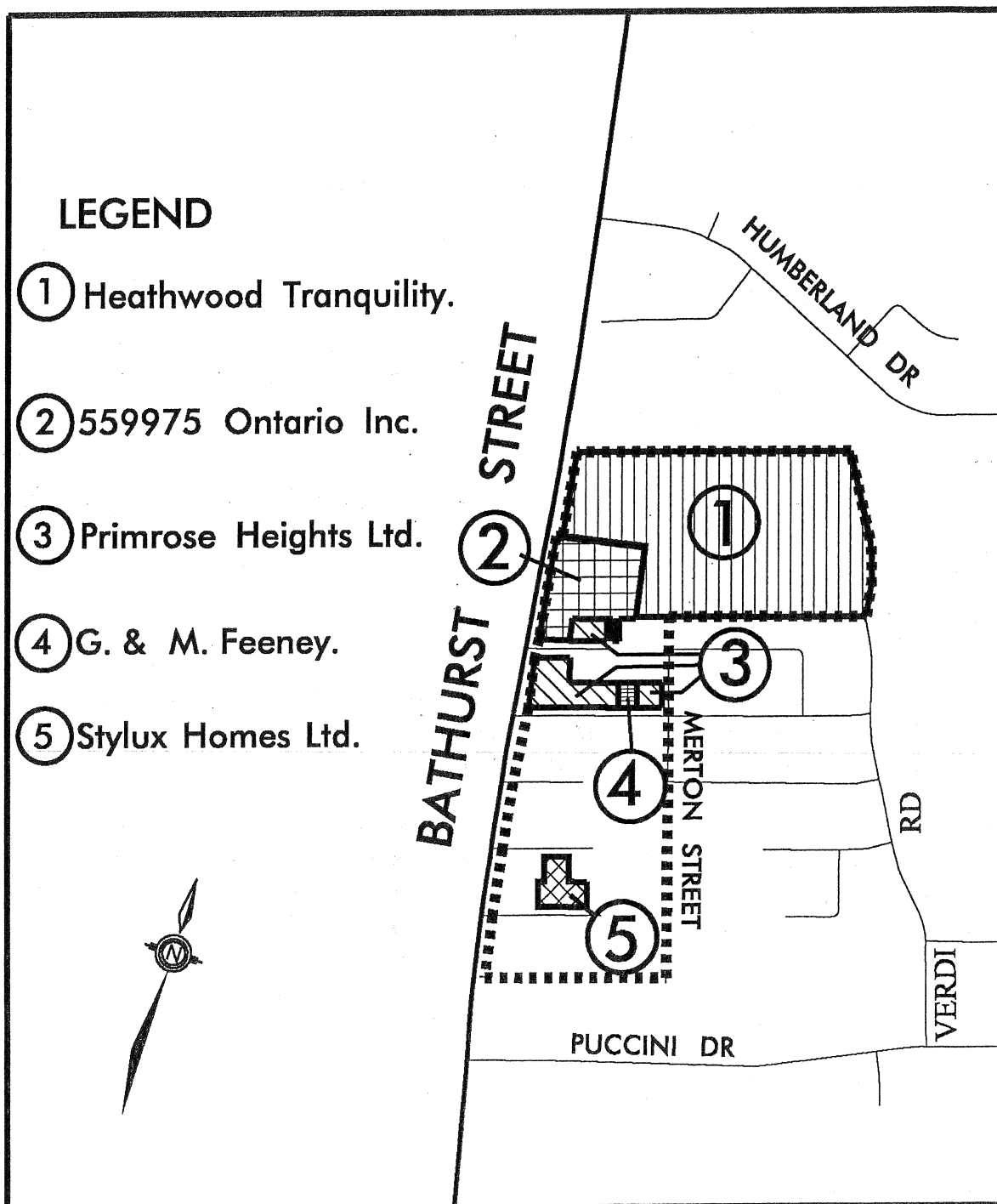
Submitted by:

Approved by:

Ana Bassios
Commissioner of Planning and Development

M. Joan Anderton
Chief Administrative Officer

Location Map



BACKGROUND:

Official Plan Amendment No. 129 designates various infill areas within the existing residential areas of the North Urban Development Area (Shown on Map 1). For each infill area, the policies within OPA No. 129 require that Council approve comprehensive concept plans prior to the development and redevelopment of these areas. Specifically, the policy requires that:

“ comprehensive concept plans for infill areas shall be approved by Council prior to the amendment of the Zoning By-law and consideration of applications for development on individual parcels.”

In 1997, the consulting firm of The Planning Partnership was retained to prepare Infill Concept Plans for the Puccini Drive and Bathurst Street neighbourhoods. After an extensive public consultation process, the Bathurst and Puccini Neighbourhoods Infill Study was completed and approved by Council on June 15, 1998. In 1998 and 1999, Council also approved infill studies for the Bond Crescent, Douglas Road and Elm Grove/ Maple Grove/ Aubrey Avenue neighbourhoods.

While infill development and redevelopment has generally proceeded within these other neighbourhoods in accordance with the studies, including the Puccini Drive neighbourhood, only minimal redevelopment has occurred within the Bathurst Street neighbourhood to date.

In 2003 and 2004, Council approved rezoning applications affecting three small parcels of land within the southerly part of the neighbourhood. These applications, on Laurier, Portage and Prince Arthur Avenues, were to permit three semi-detached and seven single detached lots. Homes have now been built on each of these properties. There was also a rezoning application approved in 2003 for nine townhouse dwellings on Bathurst Street with rear lane access; however, development of this property has not proceeded.

Applications for zoning by-law amendments have also been submitted for the development of four separate but abutting properties within the northerly portion of the neighbourhood. These four proposals, shown on Maps 2-5, are for single detached, semi-detached and condominium townhouse dwellings. Staff raised concerns with the applicants regarding the lack of coordination between the projects with respect to housing forms, access interconnection and general site design. The affected owners were requested to work cooperatively to achieve a more coordinated and comprehensive approach to redevelopment of their lands and resolve design conflicts. Although there were discussions among the owners in this regard, the discussions have resulted in the submission of just one revised proposal (the Heathwood (Tranquility) draft plan – Map 2).

In May of 2005, Planning Staff requested The Planning Partnership to revisit the design guidelines of their 1998 infill study, with the goal of providing more detailed design options for new development and redevelopment of this neighbourhood. It was felt that by providing various development options, at a greater level of detail, there would be more impetus for cooperation among the owners, resulting in more comprehensive designs and land use and development efficiencies.

An additional benefit would be the production of an up-to-date study incorporating urban design principles and guidelines for the proposed residential uses, for endorsement by Council.

On March 2, 2006, the (then) Ward 1 Councillor hosted a residents meeting to inform local residents of the development applications being considered within the study area, and to present a draft of the study for information and discussion. The meeting was attended by about 60 persons and included those living in the study area itself, in the Monarch subdivision to the east and in the Humberland Drive area to the north. The concerns expressed by residents related to the proposed development by Heathwood Homes (Tranquility) Ltd. for the lands south of Humberland Drive, adjacent to the East Humber River and the new road connection to Bathurst Street proposed by the infill study. The study attached as Appendix "A" is essentially the same as the document circulated at the residents meeting. Only minor edits have been made to the text and graphics for clarification purposes.

The Study Area

The Bathurst Street neighbourhood is shown on Map 1, which is Schedule "B" – Community Structure of the North Urban Area Secondary Plan (OPA No. 129). This neighbourhood is an older, suburban area and, in terms of infill redevelopment, contains two distinct development areas, known as the existing neighbourhood and the "vacant lands" area.

The existing neighbourhood is comprised of primarily 35 to 50 foot lot single detached houses on dead-end local roads which include Prince Arthur Avenue, Portage Avenue, Laurier Avenue and Lowther Avenue. Madison Avenue is constructed from Bathurst Street through to a new subdivision further east developed by Monarch Construction. There are vacant residential lots interspersed with existing houses throughout the existing neighbourhood.

The "vacant lands" area is located between the existing neighbourhood and the Monarch subdivision to the east. While these lands are predominantly vacant, they were originally subdivided into 7.6 metre (25 feet) single detached lots which do not meet the requirements of the existing zoning by-law and are held in multiple ownerships. This previous subdivision plan, registered in 1914, has been "deemed" not to be a plan of subdivision under the *Planning Act*. As a result, the size of individual holdings in this area varies considerably, depending on the number of individual lots that have come into single ownership. The Town owns the unopened road allowances in the area and has also acquired various properties through tax arrears. Eventually, the Town may dispose of its lands but the process for doing so must include consideration of land use patterns, construction of the unopened road allowances to Town standards and integration of the grading and drainage systems between Bathurst Street and the East Humber River.

Development of the Monarch subdivision to the east has resulted in the construction of roads and services which will eventually require connection to the existing road allowances within the Bathurst Street Neighbourhood. As it will be necessary to properly coordinate new development occurring within the "vacant lands" area and the Monarch subdivision, all development proposals will have to conform to a comprehensive grading, drainage and servicing plan. A grading and servicing review was undertaken in 1999, which provides engineering guidelines for development and redevelopment in the neighbourhood.

It will be necessary for the vacant lands in this area to be developed as comprehensive blocks by plan of subdivision, which will also provide for construction of the unimproved road allowances to Town standards.

Highlights of the Hughey West Infill Study

The purpose of this infill study and its guidelines is to provide comprehensive direction on how the neighbourhood could develop over time. In addition, it is anticipated that the study will initiate discussion among neighbouring landowners in the area and is to be followed when designing specific proposals. Town Staff will use the study as a guide when reviewing development applications in the area.

The 1998 infill study for the Bathurst Street neighbourhood contained the following key principles. The "Demonstration Plan" from this study is attached as Map 7.

- Create neighbourhood focus through the establishment of parkland, stormwater management facilities and open space.
- Allow for a transition in densities from medium density along the Bathurst Street frontage to predominantly single detached lots adjacent to the Elmway (Monarch) subdivision. The minimum single detached lot size permitted is 10.5 metres (34.4 feet). Semi-detached lots were not envisioned.
- Maintain the existing grid system of streets with reduced numbers of access points to Bathurst Street.

The Hughey West study differs from the previous study in the following manner:

- It provides a number of design concepts for development or redevelopment within the neighbourhood. The infill study includes several illustrated concepts to explain various opportunities within the area.
- It envisions single detached, semi-detached and townhouse forms of housing throughout the area.
- It provides for both condominium and freehold townhouses, which could locate on either the existing street system, on a new north-south street to the west of Merton Street or on lands that would include the road allowances of the streets that will ultimately be closed at Bathurst Street.
- It provides for a new street connecting the most northerly part of the neighbourhood to Bathurst Street. With the future closure of Lowther Avenue at Bathurst Street, it will serve as a collector spine that will accommodate north end residents who intend to travel northbound on Bathurst Street, without first having to travel down to Madison Avenue to access Bathurst Street.

- It differentiates between the “entrance streets”, which will provide permanent access roads into the neighbourhood and to the Monarch subdivision to the east, and the three remaining “interior streets” which are intended to be closed at Bathurst Street in the future. Closing the interior streets will reduce the number of roads intersecting Bathurst and will increase the spacing of the streets that will remain open. On the entrance streets, housing is restricted to singles with a minimum frontage of 12.0 metres (39.4 feet), semis with a minimum frontage of 18.0 metres (59.1 feet) or rear lane townhouses. The purpose of these restrictions is to limit the number of driveways, which would otherwise dominate the streetscape and impede the vehicle stacking requirements of these streets as they approach Bathurst Street. Closer spaced driveways also limit the availability of on-street parking.

The interior streets can be developed with 10.5 metre (34.4 feet) singles, 14.6 metre (47.9 feet) semis, 6.0 metre (19.7 feet) street townhouses or rear lane townhouses.

- It contains detailed urban design and architectural control guidelines for new development or redevelopment in the study area. The guidelines are independent of the type of use, extent of assembly and timing of development. The guidelines deal with the manner in which housing addresses particular streets, such as frontage, flankage, setbacks and landscaping.

Development Applications

At the time of preparation of this report, there are five development proposals within the study area:

1. File Nos. D02-95039 and D03-95121
Heathwood Homes (Tranquility) Ltd.

This development, shown on Map 2, is for a total of 129 single detached homes to the south of Humberland Drive, between Bathurst Street and Coons Road. Only the most westerly part of the development is within the Hughey West Infill Study, consisting of 64 single detached lots with frontages of 10.7 metres (35.1 feet) and 12.2 metres (40 feet). The draft plan of subdivision has been revised to reflect the design goals of the infill study, particularly the provision of a road stub to facilitate a road connection westerly to Bathurst Street.

2. File Nos. D02-03070 and D03-03022
Primrose Heights Ltd.

This development, shown on Map 4, is for 13 semi-detached lots (26 units) and one residential reserve block. The lots are located on both sides of Lowther Avenue and the north side of Madison Avenue. The reserve block is located on Madison Avenue, directly west of “Lot 15”. The reserve block is intended to be combined with the site to the west (shaded on Map 4), which is under separate ownership, and be developed either jointly or through land acquisition by either party. However, the site to the west is subject to a separate rezoning application (see No. 4 below) for two single detached lots.

This conflicts with the proposal by Primrose Heights for semis. Staff has requested the two owners to resolve their conflicting plans.

3. File Nos. D02-04003 and D06-04010
559975 Ontario Inc.

This development, shown on Map 3, is for 40 block (condominium) townhouse units. The property is located on the east side of Bathurst Street, between Lowther Avenue and the East Humber River and contains a former elementary school.

While it is surrounded on three sides by the two development proposals referred to above, there is no interconnection proposed with the adjoining lands. Its access is proposed from Bathurst Street and Lowther Avenue.

In response to the draft version of the Hughey West Infill Study made available at the residents meeting in 2006, this applicant has submitted, in concept form, a revised proposal which provides the new public road envisioned by the infill study. Although a number of revisions would be required to comply with the infill study, Staff are encouraged that the owner has indicated willingness to work toward meeting its goals.

4. File No. D02-05050
G. and M. Feeney

This proposal, shown on Map 5, is a rezoning application to permit an existing vacant lot, which has a frontage of 22.9 metres (75 feet) on the north side of Madison Avenue, to be severed in half to create two single detached lots. The property abuts the lands owned by Primrose Heights Ltd. on its east and west sides (refer to No. 2 above). The Feeney proposal conflicts with the proposal by Primrose Heights Ltd. for semis. Staff has requested the two owners to resolve their conflicting plans. In addition, the Feeney proposal would create single detached lots that are smaller than the minimum 12.0 metre (39.4 feet) frontage envisioned by the infill study for new lots on Madison Avenue, an "entrance street".

The owners recently appealed their rezoning application to the Ontario Municipal Board and a hearing is scheduled for June 21, 2007. Staff intends to schedule a Council Public Meeting for this application prior to the OMB hearing, to give direction to staff at the hearing.

5. File No. D02-07006
Stylux Homes Ltd.

This development, shown on Map 6, is for five semi-detached lots (10 units) and one single detached lot. The property is known as 24 Prince Arthur Avenue and has frontage on both Prince Arthur and Portage Avenues. The concept plan has been designed to comply with the provisions of the Hughey West Infill Study.

Departmental Comments

The infill study has been reviewed by the Engineering and Public Works and Parks, Recreation and Culture Departments. Their comments are generally supportive of the goals and design options set out in the study. Individual development applications will be assessed in relation to the Town standards and policies normally administered by those Departments.

Implementation

During the original consultation process leading to the Bathurst and Puccini Neighbourhoods Infill Study, there were questions regarding implementation of infill development, including the need for adjacent property owners to cooperate to achieve a lotting pattern consistent with the lot size recommendations in the infill study, as well as the process for development approvals after the infill study is approved by Council. It was determined at that time that applications for rezoning, subdivision approval or consent could proceed following approval of the study, provided that where appropriate, owners cooperate and file applications that are comprehensive. Applications must generally conform to the key principles and design guidelines of the study, as well as OPA No. 129 and applicable Town and Regional design standards.

The implementation process is still the same with respect to the Hughey West Infill Study, provided however, that the approval of individual applications must conform with current policies or direction of Council regarding the availability of servicing capacity, the use of Holding provisions in zoning by-laws and further residential approvals in general.

Provincial Policy Statement

The 2005 Provincial Policy Statement (PPS) came into effect on March 1, 2005. The Planning Act states that municipal decisions "shall be consistent with" Provincial Policy Statements (PPS). The PPS focuses growth within settlement areas in efficient development patterns and promotes healthy, livable and safe communities by accommodating an appropriate range and mix of uses to meet long term needs. It also states that municipal Official Plans are the most important vehicle of implementation and that Official plans shall provide clear policies to direct development to suitable areas.

Conformity with the *OAK RIDGES MORAINÉ PROTECTION ACT* and the *OAK RIDGES MORAINÉ CONSERVATION PLAN*

The lands within this infill area are within the Settlement Area designation of the Oak Ridges Moraine Conservation Plan. The purpose of this designation is to focus and contain urban growth. Section 18(3) of the Conservation Plan states that all uses permitted by the applicable Official Plan are permitted, subject to compliance with specific environmental provisions in the Plan. Individual development applications are assessed in relation to the provisions of the PPS, OPA No. 129, the *Oak Ridges Moraine Conservation Act, 2001* and the Oak Ridges Moraine Conservation Plan.

FINANCIAL/STAFFING/OTHER IMPLICATIONS:

The recommendation does not have any financial or other implications.

RELATIONSHIP TO THE STRATEGIC PLAN:

There are no direct implications with respect to the Strategic Plan.

CONCLUSIONS:

The Hughey West Infill Study attached as Appendix "A" to this report, was commissioned with the goal of providing more detailed design options for new development and redevelopment of the neighbourhood. By providing various development options, at a greater level of detail, there should be more impetus for cooperation among the owners. An added benefit of increased cooperation is a greater potential for land use efficiencies and overall better designed plans. The study also includes current urban design guidelines that were not part of the original infill study for the Bathurst Street neighbourhood.

The Hughey West Infill Study continues to meet the requirements of OPA No. 129 for the preparation of a comprehensive infill plan for the Bathurst Street neighbourhood. It is also consistent with the Provincial Policy Statement and conforms to the *Oak Ridges Moraine Conservation Act* and Conservation Plan. It is recommended that Committee of the Whole recommend to Council that this new infill study and its Urban Design Guidelines be endorsed as the basis for considering future development in the Bathurst Street neighbourhood, by augmenting the existing 1998 infill study.

APPENDIX CONTENTS

“A” Hughey West Infill Study and Urban Design / Architectural Control Guidelines

Drawings

- Map 1** - Schedule “B” of OPA No. 129 – Community Structure
- Map 2** - Proposed Zoning By-law Amendment and Draft Plan of Subdivision
D02-95039 and 19T(R)-95121 (Heathwood Homes (Tranquility) Ltd.)
- Map 3** - Proposed Zoning By-law Amendment and Site Plan Application
D02- 04003 and D06-04010 (559975 Ontario Inc.)
- Map 4** - Proposed Zoning By-law Amendment and Draft Plan of Subdivision
D02-03070 and 19T(R)-03022 (Primrose Heights Ltd.)
- Map 5** - Proposed Zoning By-law Amendment Application
D02-05050 (G. & M. Feeney)
- Map 6** - Proposed Zoning By-law Amendment Application
D02-07006 (Stylux Homes Ltd.)
- Map 7** - Bathurst Street Neighbourhood Demonstration Plan
1998 Puccini and Bathurst Neighbourhoods Infill Study











INFILL - RESIDENTIAL STUDY AREAS

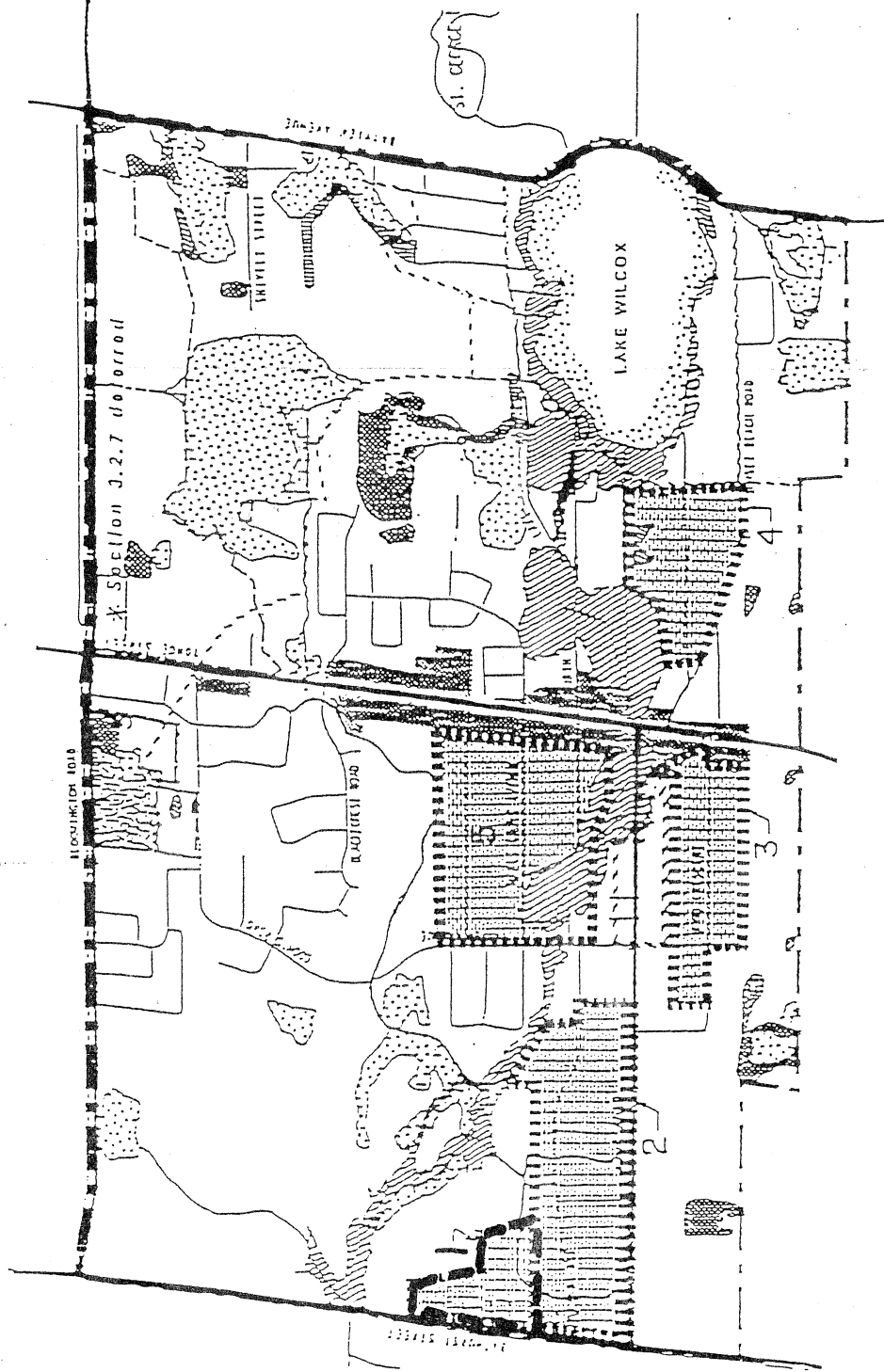
LAKE WILCOX
OAK RIDGES

Town of Richmond Hill
Schedule B




Community Structure

Legend

-  Community Care
-  King Rd. Commercial Area
-  Infill
-  Development Area
-  Hazard Land
-  Environmental Protection Area 1
-  Environmental Protection Area 2
-  Arterial Roads Under Provincial or Regional Jurisdiction
-  Land Form Conservation (Subject to Section 3.2.7)
-  OPA 129 Boundary



STUDY AREAS

-  1 BATHURST STREET NEIGHBOURHOOD
-  2 PUCCINI DRIVE NEIGHBOURHOOD
-  3 BOND CRESCENT NEIGHBOURHOOD
-  4 DOUGLAS ROAD NEIGHBOURHOOD
-  5 ELM GROVE / MAPLE GROVE / AUBREY AVENUE NEIGHBOURHOOD

BLOCK 1 & 2 FILE NO. D18-07003

**TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT**

BR/SS SRPD.07.030 MAP 1

PART 1
PLAN 65R-20930
PIN 03206-1198(LT)



D02-04003 / D06-04010

559975 Ontario Inc.

BR/SS SRPD.07.030 MAP 3

BLOCK 01

FILE NO.D18-HU

**TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT**

PART 6
PLAN 65R-3563

PART OF LOT 66 CONCESSION 1 W.Y.S.
PM 0324-007

LOT 67
CONCESSION 1 W.Y.S.

OTHER LANDS OWNED BY OWNER

LOWTHER AVENUE
(BY REGISTERED PLAN 133)

PM 0324-204

158.5850 N733230°E

RIDEAU STREET
(BY REGISTERED PLAN 133)

BATHURST STREET - REGION ROAD No. 38
(MADE BY BR/SS PM 6-670-00-126-957 (17152))
ROAD ALLOWANCE BETWEEN CONCESSION 1 W.Y.S. AND CONCESSION 2 (KIND)
PM 6504-001



MADISON AVENUE
(BY REGISTERED PLAN 133)

LOTS 954 -- 956

D02-03070 / D03-03022
Primrose Heights Ltd

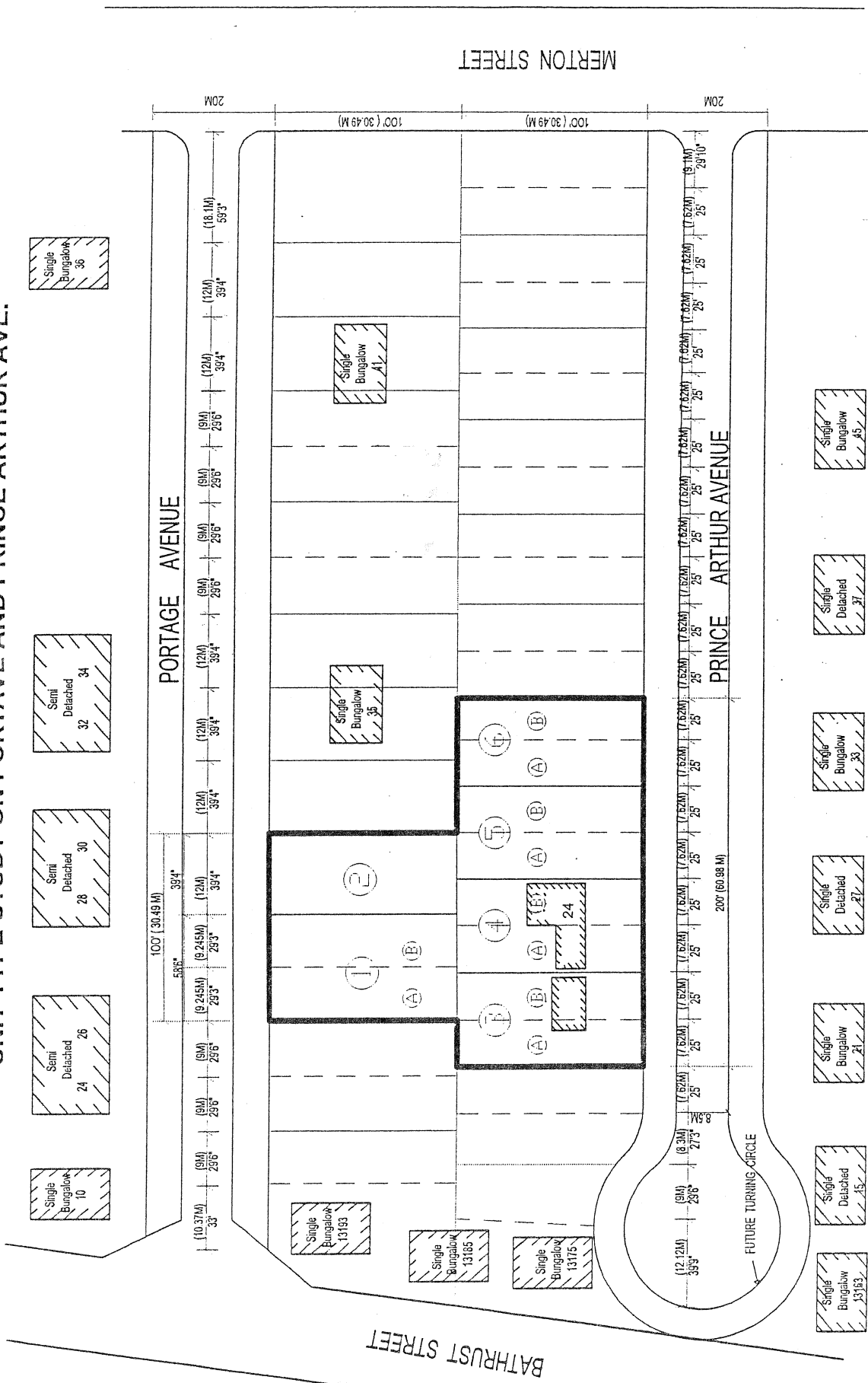
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BLOCK 01

**TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT**

BR/SS SRPD.07.030 MAP 4

UNIT TYPE STUDY ON PORTAVE AND PRINCE ARTHUR AVE.



D02-07006

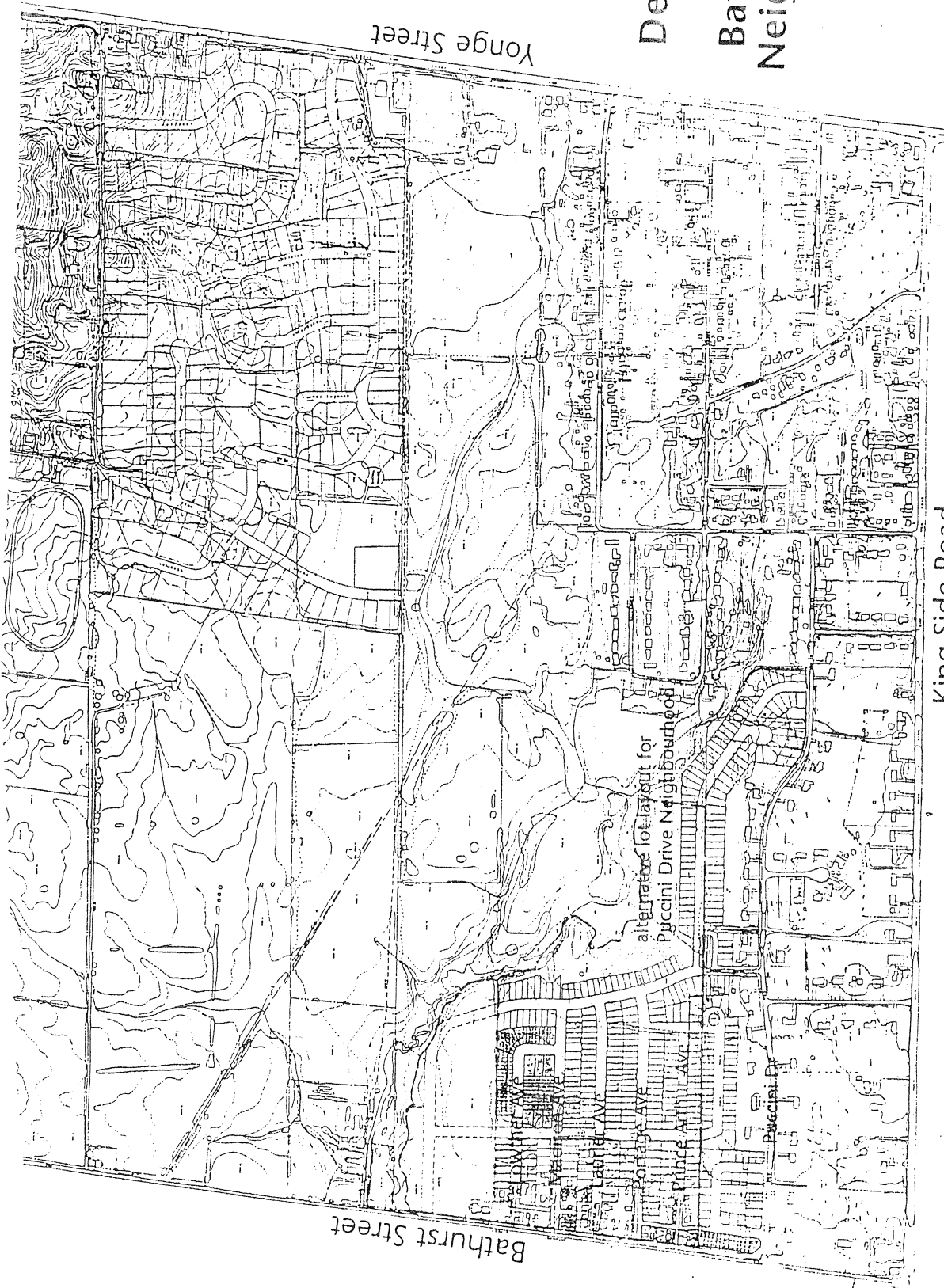
Stylux Homes Ltd

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BLOCK 01

**TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT**

BR/SS SRPD.07.030 MAP 6



LEGEND

- low density residential
- medium density residential
- open space

Demonstration Plan Bathurst Street Neighbourhood

BATHURST STREET NEIGHBOURHOOD

Demonstration Plan

FILE NO. D18-HU

BLOCK 01

TOWN OF RICHMOND HILL
PLANNING & DEVELOPMENT

BR/SS SRPD.07.030 MAP 7

HUGHEY WEST STUDY AREA

TOWN OF RICHMOND HILL

MARCH 2007

PREPARED BY: THE PLANNING PARTNERSHIP

PART A - HUGHEY WEST INFILL STUDY

INTRODUCTION

HUGHEY WEST: TOWN OF RICHMOND HILL

The Hughey study area is bounded by Bathurst Street on the west, Prince Arthur Avenue on the south, Verdri Road on the east and the valley and conservation lands of the East Humber River on the north and east. The entire area except for the most northerly sector has been laid out with registered plans which are a grid of east west and north south streets and this consideration is critical to our report recommendations for the future infill of the undeveloped portions of this area.

The study area is roughly divided into 2 sectors - developed and developing, and as yet primarily undeveloped. The divide is marked by Merton Street formerly known as Rideau Street.

Responding to a request from the Region, the municipality is proposing to close alternate east west streets to the north of Puccini: these are Prince Arthur Drive, Laurier Avenue, and Lowther Avenue. (see diagram at left)

North of Lowther Avenue are two major proposals which cover the balance of the study area to the north: a proposal for 40 condominium town housing units on the lands fronting on Bathurst Street and for 60 single family units on the balance of the northerly lands. While these 2 proposals are adjacent, there is no connection proposed to link them in any way and this study considers this an important deficit to be addressed. (see diagram at right)

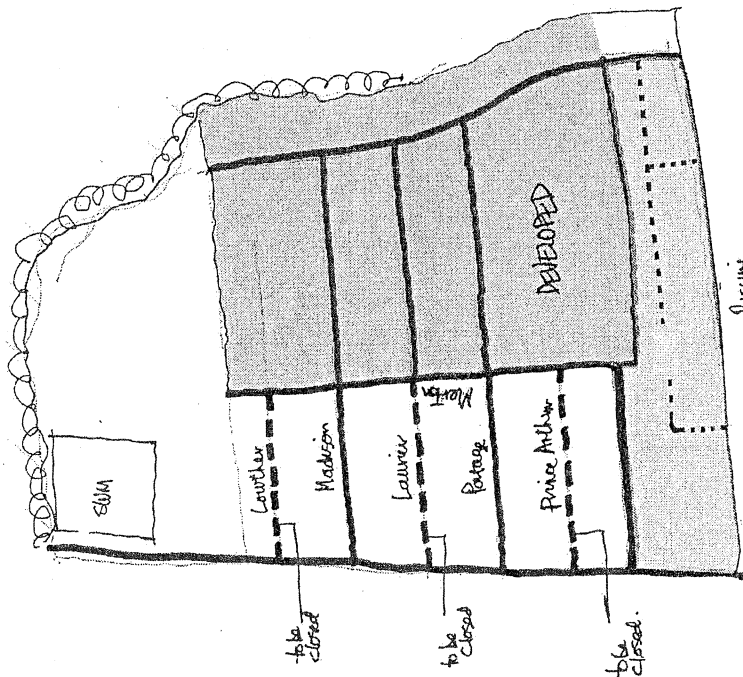


fig.1

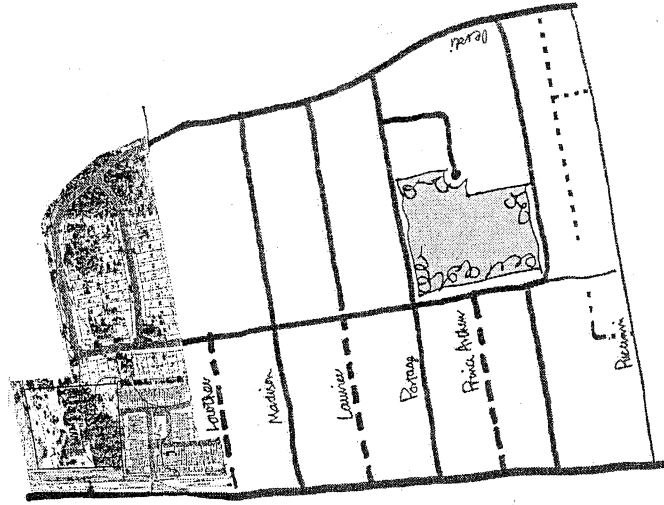


fig.2

HUGHEY WEST: TOWN OF RICHMOND HILL

At the commencement of the study we were given a compendium of proposals from the various developers and from the municipality, all at different scales, addressing the specific and immediate concerns of the private and public interests. This was endemic to the planning problem for the area which we felt our study had to address: there had been no comprehensive consideration for the entire area which related independent proposals to each other and to the area as a whole including the already developed portion.

A preliminary master plan for the undeveloped portion had been prepared for the municipality and developers by others. The plan was based on the speculation that the balance of infill would primarily occur as condominium development. The underlying problem of projecting this development form, to be appropriate for virtually the entire balance of the study area, is that in order to do a plan of that nature one has to speculate on 3 major factors: how land will be assembled and on what scale, in what order will development applications come in (creating say, opportunities for alternate access between existing streets) and, as well, whether the developer of any particular assembly will want to create condo or freehold lots.

It is our opinion that in an area where ownership is highly fractured and the majority of outstanding proposals are on small assemblies, a much more feasible form of development will occur as freehold, particularly because of the underlying existing and dedicated street grid.

Our initial approach was to reevaluate outstanding plans in the north end of the study area with the intention of improving access between development "cells" as well as attempting to improve overall urban design considerations. This is illustrated in Figure 3. The most important aspect of this examination was to realign the north end of Verdi Road through the proposed single detached residential area so that it serves as a collector spine through the east of the study area. To accommodate north end Hughey residents who intend to travel northbound on Bathurst Street, Verdi Road should continue westerly to intersect Bathurst Street, which would require the approval of York Region.

In addition to the proposed access between the two developments, the preliminary plan was also changed so that the major "entry streets" into the area were developed either with single family units or with condominium town houses with rear lane access. The rationale for this change is further elucidated in the following section.

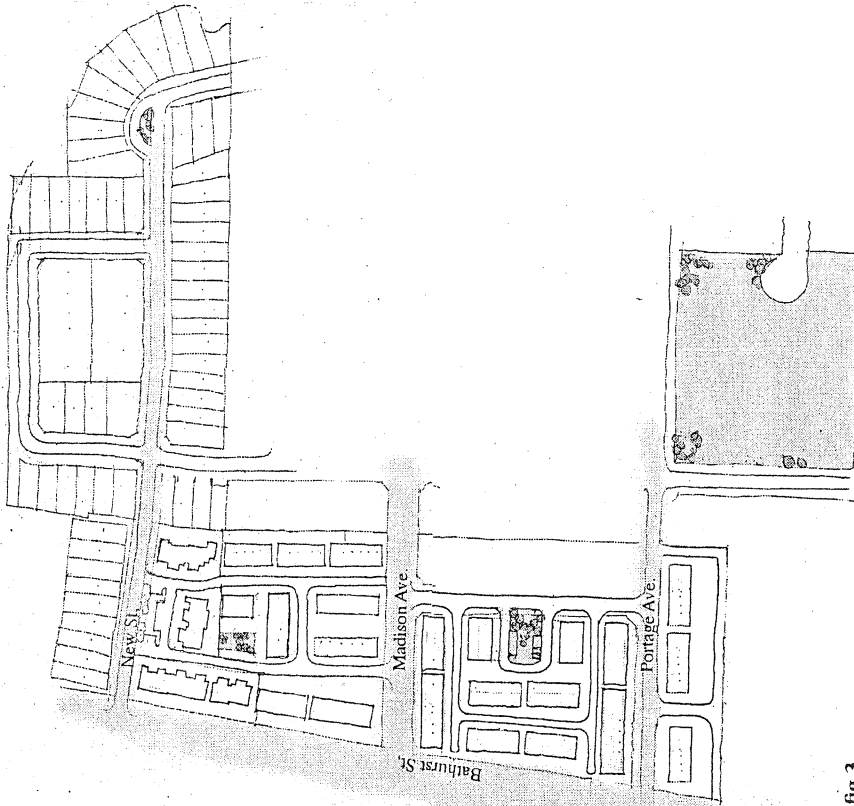


fig.3

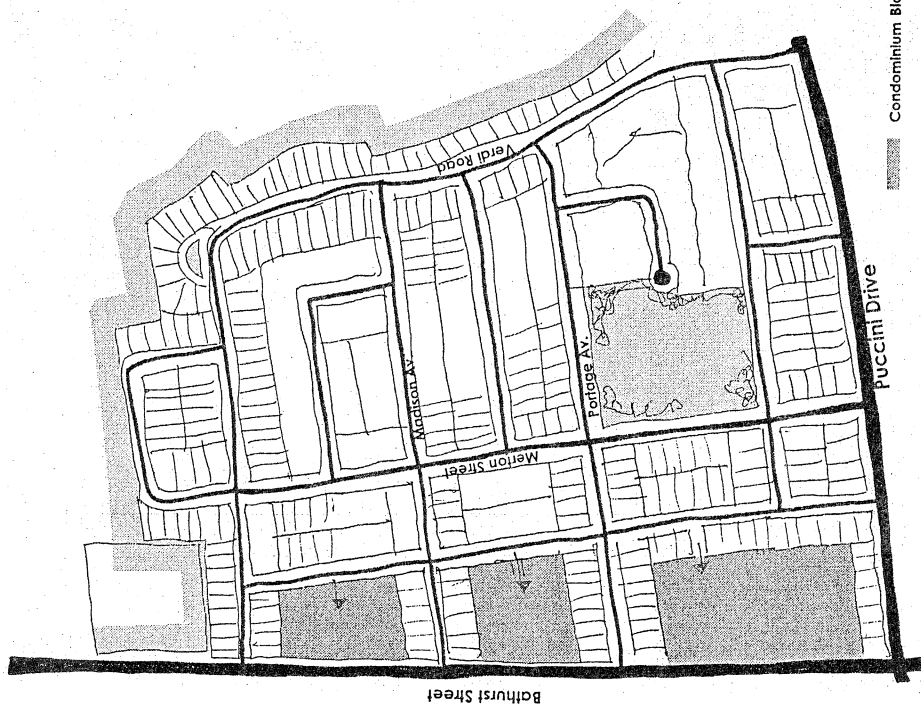


fig.4a: Concept A

The difficulty with existing or potential proposals which project town housing forms on the roads that access Bathurst: specifically Portage, Madison and our proposed Verdri Rd extension is the plethora of driveways that would dominate these access routes. This is both a functional and an urban design concern. The municipality does not currently permit public lanes; in lieu, in order to obviate driveways, town housing development on these roads will demand condominium proposals and the problems of that have been discussed above.

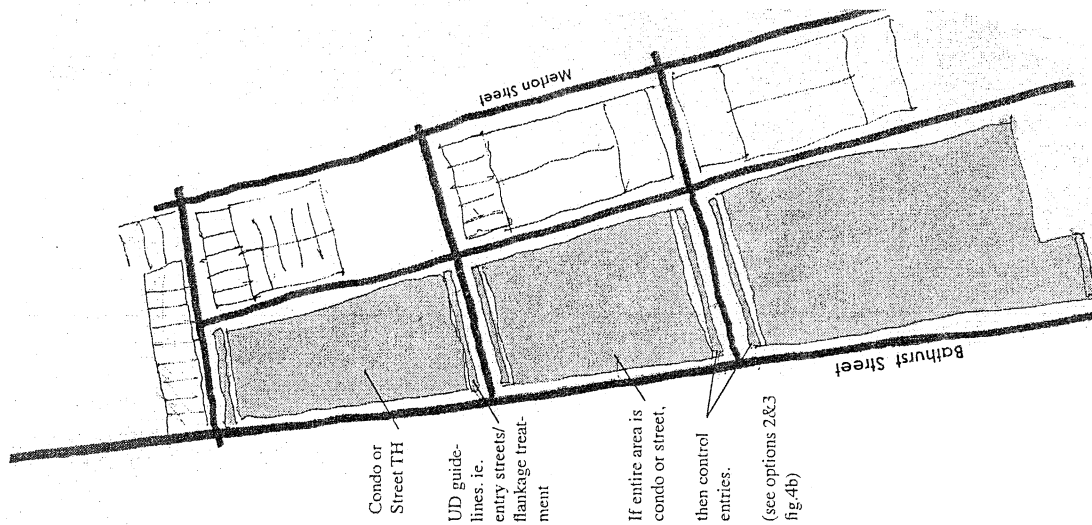
Figure 4a indicates an alternative which would propose single family housing on the "approach" roads and condominium forms in the interstitial areas of the closed streets. Figure 4b illustrates the completed street pattern which would create the highest unit yield of this proposition- the condo streets are terminated in bulbs, permitting the maximum pedestrian access to Bathurst and the transit that it will accommodate. An additional feature of this alternative is the proposal of a new north-south road a development block west of Merton: its purpose is to reduce the amount of assembly necessary to develop the condominium areas.

In all cases housing on the "entrance" streets are proposed as either single detached with a minimum of 12m frontage or as semi-detached units with paired driveways with a minimum of 18m. Frontage on interior streets, single detached units will have a minimum frontage of 10.5 m, semi-detached units a minimum of 14.6m and street townhousing a minimum of 6.0m.

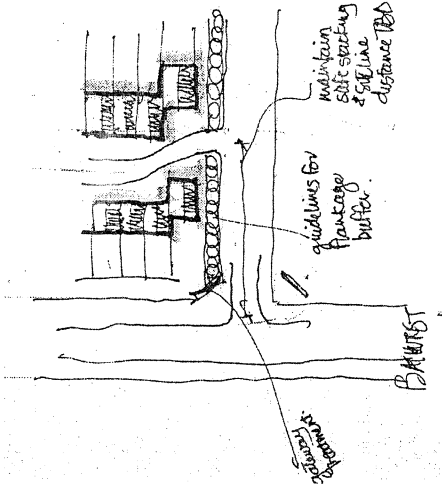


fig.4b: Concept B

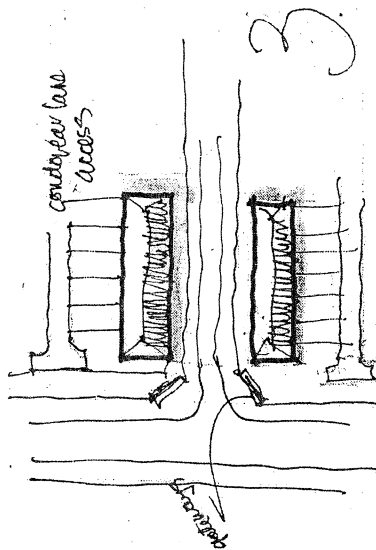
HUGHEY WEST: TOWN OF RICHMOND HILL



- Option 1:
1. all entry roads are same family, minimum flankage 12. m
 2. driveways minimized
 3. condos behind (or ahead if)
 4. gateway L-A. Treatment
 5. corner unit, design control.



- Option 2
1. multiple developed along entry road to be flanking related
 2. maintain 1A buffer guidelines to specify
 3. gateway treatment



- Option 3
- or where flanking on entry roads multiple housing forms to have rear lane access

fig. 5b

Figures 5a and 5b (3 options) illustrate the design standards that could be applied to either areas where assembly would propose entire blocks of condominium, or a mixture of condo and freehold so that the problems of driveways on the "approach" roads would be addressed.

fig. 5a

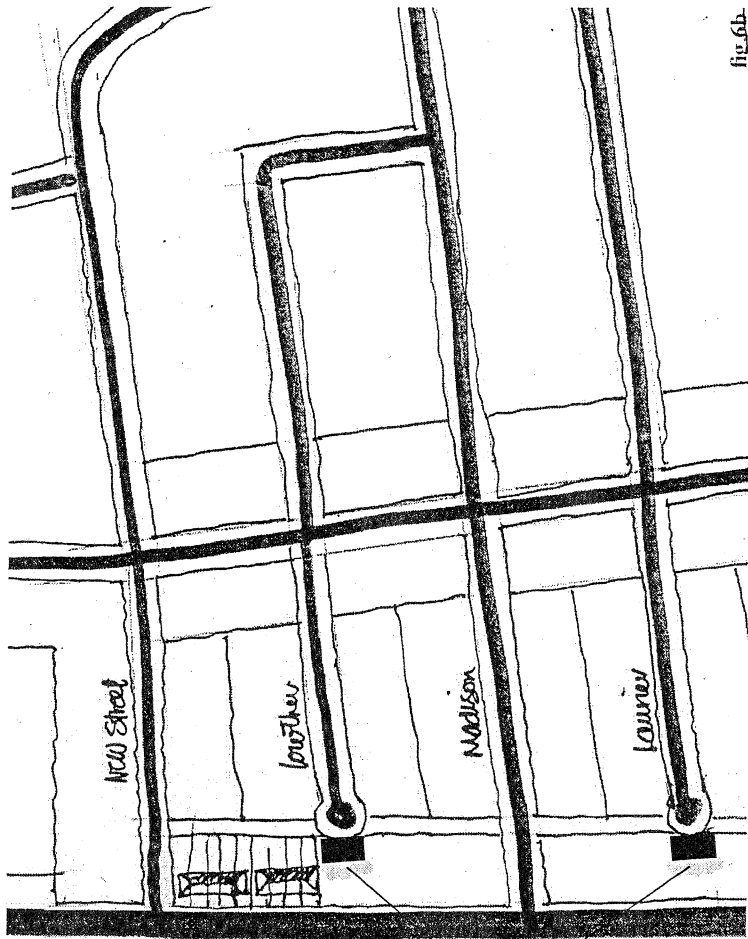


fig. 6a

Figure 6a demonstrates a plan which turns the closed streets into loop streets : where they rejoin the "approach" roads they jog in order to permit adequate sight lines and stacking distances at their junctions. This alignment also allows for a suitable gateway landscaping treatment on the right of ways of the approach streets. The plan shows 2 ways in which the west end units could address Bathurst Street: either as frontage or flankage and we would see that either could be acceptably designed. In this example zoning would permit town housing on the non approach streets.

Figure 6b shows an alternative to the previous scheme and permits development of condominium to front on Bathurst St. which would be accessed by a private rear lane. Guest parking would be incorporated into the lotting adjacent to the cul de sac locations which would be screened from view from the Bathurst Street through landscaping and / or architectural elements .

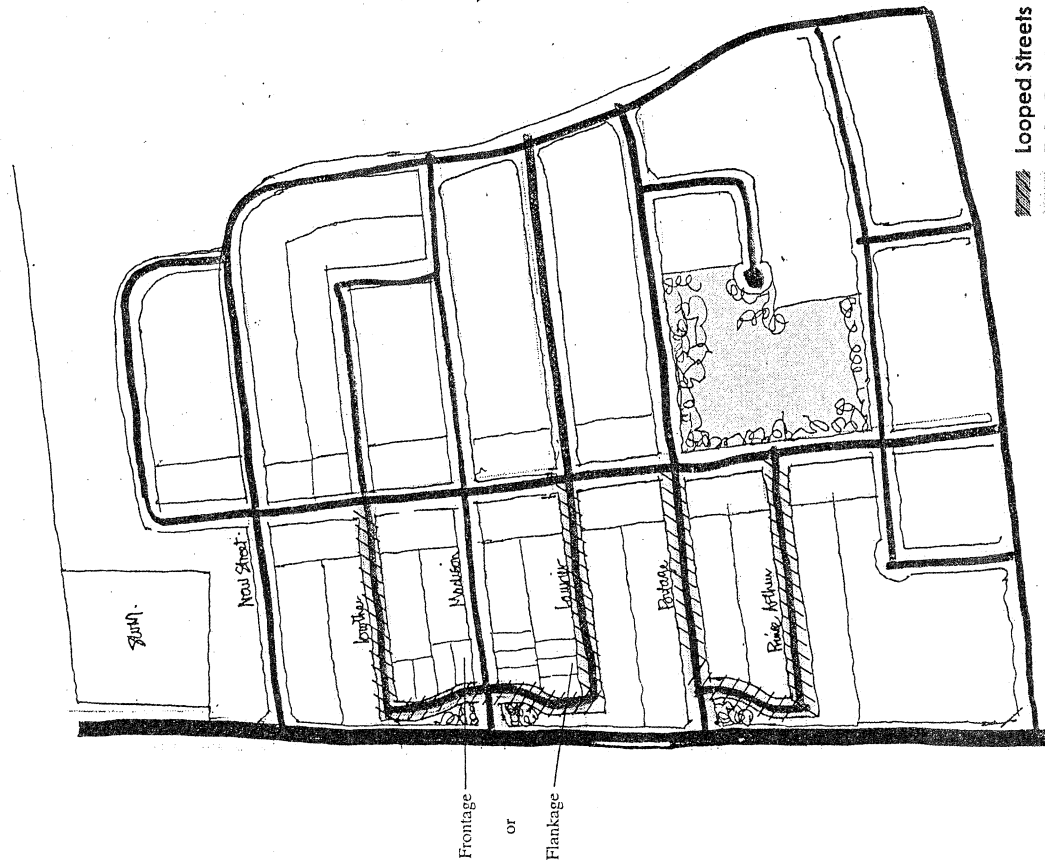


fig. 6b

HUGHEY WEST: TOWN OF RICHMOND HILL

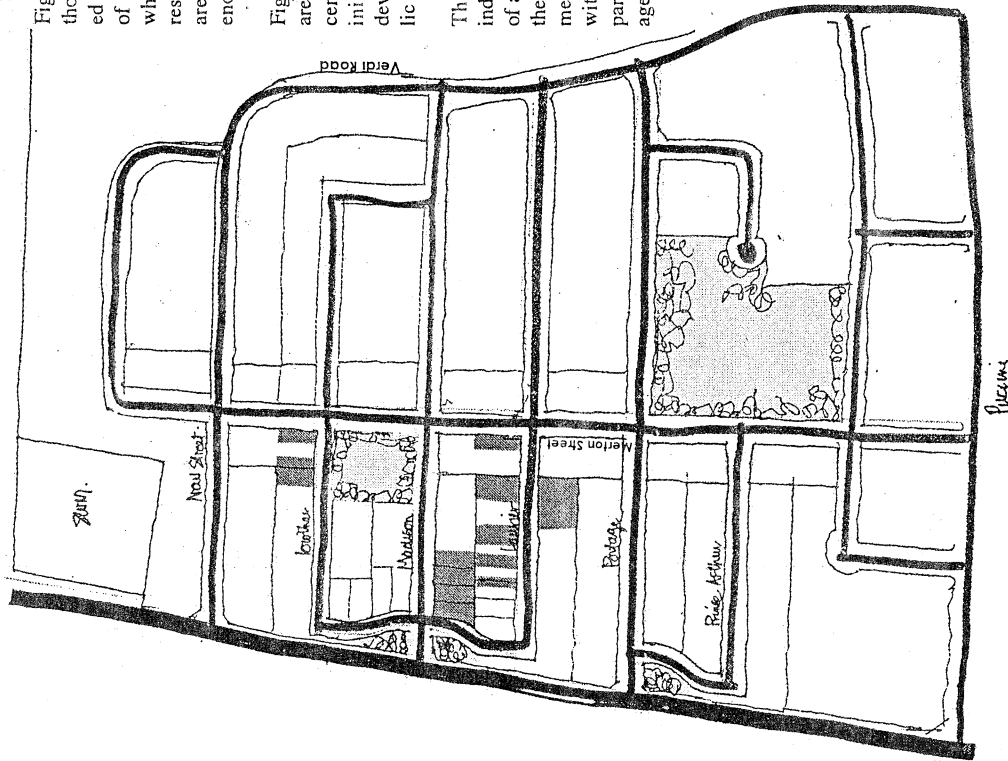


fig.7a

Figure 7a illustrates a proposal to dispose of those municipally owned lands shown shaded for the assembled lands on the west side of the intersection of Madison and Merton which would conveniently serve the residents of the northern sector of the study area as park land and break up the homogeneous and unrelieved extent of housing.

Figure 7 assembles, as Fig 7b, lands which are exchanged for a roundabout on the central approach road which attempts to initiate higher design standards for private development with an investment in the public infrastructure.

The following are guidelines that will be independent of the type of tenure, extent of assembly and the timing of applications: these rules for development (and redevelopment) are urban design guidelines that deal with the manner in which housing addresses particular streets such as frontage, flankage, setbacks, landscaping, etc.

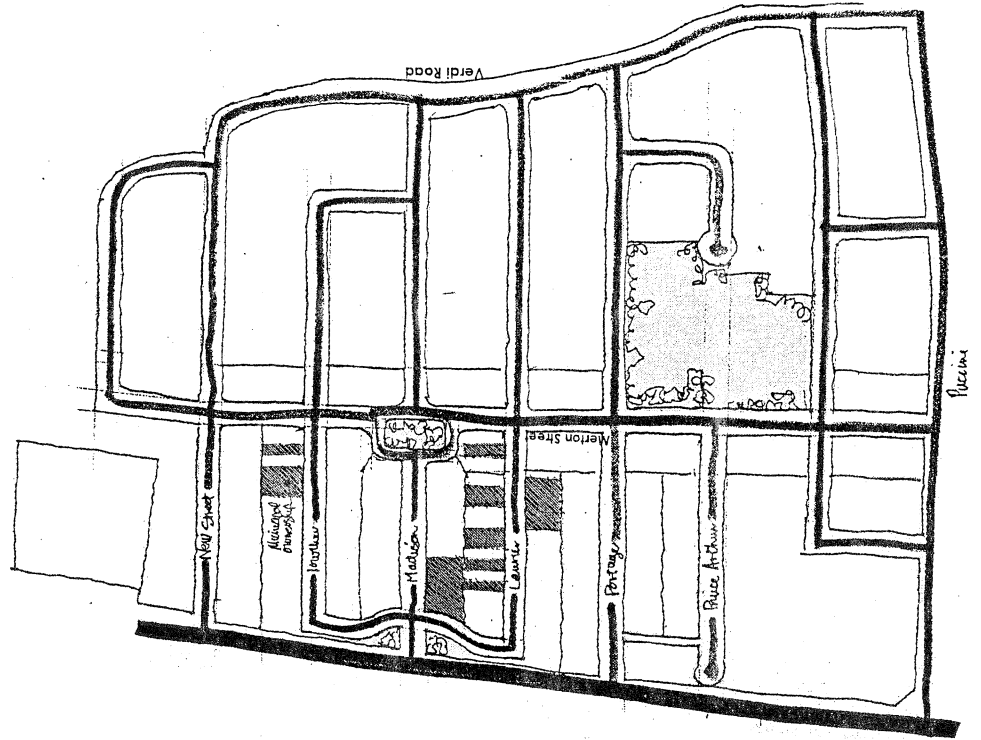


fig.7b

■ Municipal Ownership

PART B

URBAN DESIGN/ARCHITECTURAL CONTROL GUIDELINES

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THE FUNCTION OF ARCHITECTURAL CONTROL GUIDELINES

Architectural Control Guidelines can establish a method of generating a consistent level of design throughout a community. These controls should never be seen as another level of approval but rather as a method for both the municipality and the developer to set a high standard of design. These guidelines should help to develop exceptional residential communities and should act to promote innovation. Architectural controls are not zoning by-law provisions but rather should act to provide flexibility in interpreting guidelines based on site specific conditions.

Architectural controls are performance standards endorsed by municipalities to guide private development of all aspects of a proposed community. Architectural controls for the proposed Hughey West community will help development proponents and Town staff meet municipal expectations for private realm development, and will be used by the municipality to evaluate development submissions at various stages of the development review and approval process in the Town of Richmond Hill. Architectural controls will be used in conjunction with the Zoning By-law and other Town of Richmond Hill Standards, as well as the standards and procedures of the various municipal departments. The last chapter of this report details the process through which municipalities review development applications and the role of the control architect.

We have developed, over the last decade, guidelines which have been effective in creating superior communities in other jurisdictions. The following is a compendium of guidelines which we feel are appropriate to the development of the Hughey community.

1.0 DESIGN GUIDELINES FOR LOW AND MEDIUM DENSITY RESIDENTIAL DEVELOPMENT

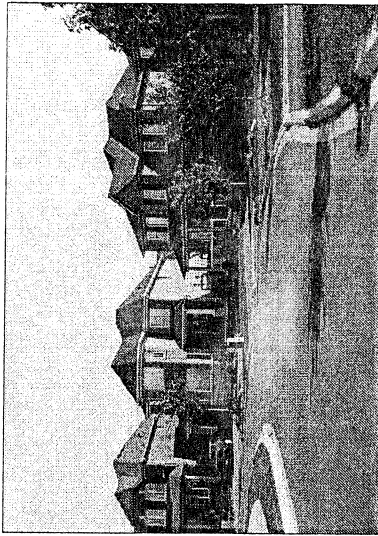
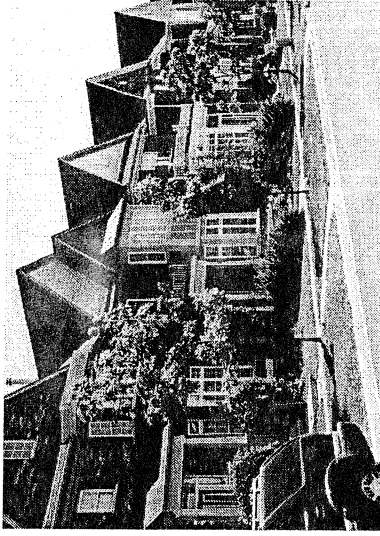
This report sets out architectural guidelines for residential development to create a cohesive and attractive neighborhood. The goal in neighbourhood design is to create livable communities with a controlled diversity. The community level guidelines relate to macro design elements including general siting, massing, as well as architectural design elements that will assure continuity throughout the neighbourhoods.

1.1 General Siting Guidelines

Appropriate siting of units contributes greatly to the streetscape and acts as a unifying element to the overall structure of the community.

- each block may/should encourage a mix of unit types with a variety of elevations;
- placement of houses in the same elevations and colour treatment next to each other is not permitted. No more than 4 identical elevations may be repeated for every 10 lots in a block, with a minimum of 3 dwellings sited between the same elevation;
- each model should have 2 distinctly different elevations, while popular models may require more than 2 elevations to avoid repetition and monotony within the streetscape;
- houses sited at the end of view corridors, such as "T" intersections, elbow streets, and on prime corner lots should be designed with upgraded architectural elevations;
- the front facade should be sited close to the front lot line;

- unit siting shall result in a door and a window or sidelights located on the front wall for all lots. Entry doors should be visible from the street, enclosure of porch or recessed entries is discouraged;
- where units are sited with side and rear elevations visible from a public space, these elevations should have a similar design quality as the front elevation. A continuation of the same materials and detailing is encouraged on all visible facades; and,
- a garage should be sited such that its front wall is projecting a maximum of 1.0m in front of the unit's front wall, or 2.0m should the unit have a front porch.



Variety in unit types, massing, and elevation design, creates an attractive streetscape

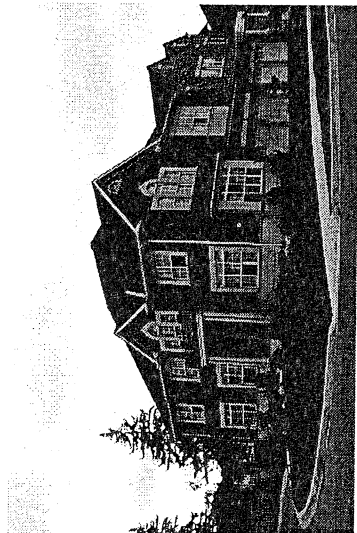
1.2 Priority Lots

The vision for the community should be reflected in the siting and design of buildings on priority lots, such as homes located at the terminus of a view corridor. In the case where a townhouse is sited on a corner lot, the end unit flanking a street is defined as a priority lot. In the case where a semi-detached dwelling (comprised of two units) is sited on a corner lot, both units are defined as a priority lot.

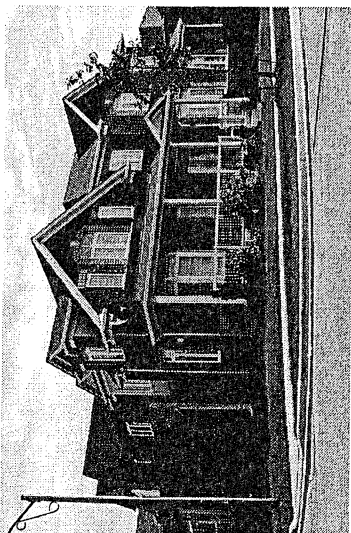
1 Gateway Lots

Principle: Homes located at entrances to the community should have architectural features to frame the entrance.

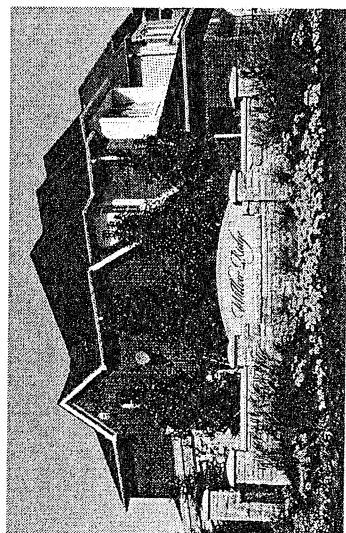
- the main entries of homes at gateways shall be located on the flanking street where feasible;
- the main entries of homes across a gateway street shall both be located on the flanking street;
- architectural features such as porches, turrets, and bay windows shall wrap the corner of the house;
- homes at gateways should include architectural features to frame the entrance to the community;
- landscape and landscape features should be provided to accentuate gateways; and,
- the architecture and landscape of the residence shall coordinate with the architecture and landscape of community entry features.



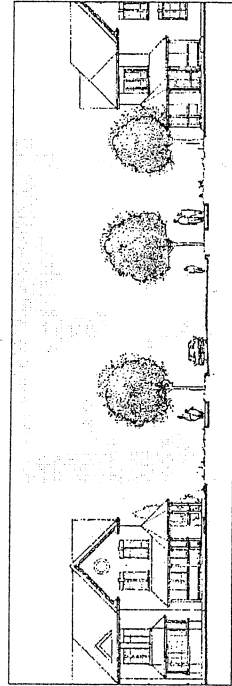
Main entry of home at gateway on the flanking street



Architecture feature such as a porch wrapping around the corner of the house



A gateway condition with a community identity feature to define the entrance



Residential buildings should form part of the gateway expression. Compatible styles and paired elements such as turrets, wrap around porches, dormers etc. add to the sense of gateway

2. Corner Lots

Principle: Equal attention will be paid to the front, exterior side, and rear elevations of a corner lot

Corner lots are those lots that occur immediately adjacent two intersecting streets. Homes on corner lots have two options for locating a front entrance and a garage. The following guidelines make full use of the opportunities inherent in a corner lot situation:

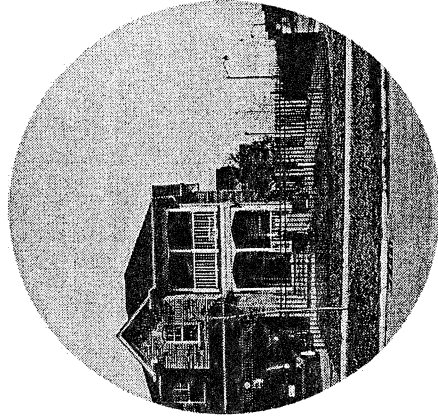
- the main entries of all homes located at corners of an intersection are encouraged on the flanking street and are encouraged on all other corners, where feasible;
- the front, exterior side, and rear elevations of corner lots will carry a consistent type and quality of materials and window treatments;
- architectural features such as a porch, turret, or bay window should wrap the corner of the house and address the corner condition;
- decorative fences shall enclose the flankage and front yards to reinforce the corner of collector roads and,
- garages shall be located away from the exterior corner of the house.

3. Units at 'T' Intersections

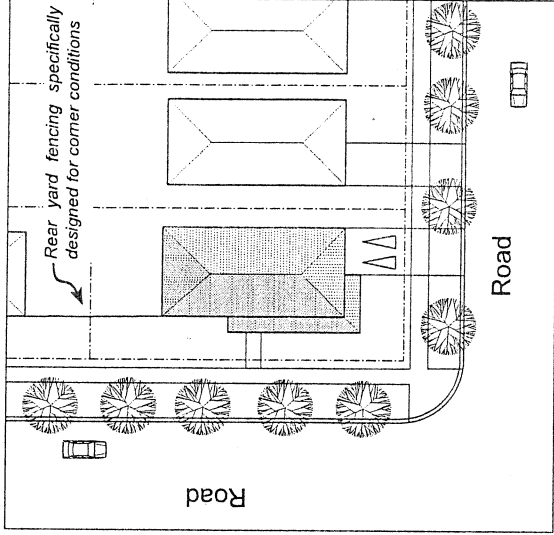
Principle: Homes at the top and facing a 'T' intersection shall be coordinated in design.

'T' intersections occur when one road terminates at right angles to another. Consideration should be given to homes at the top of the 'T' intersection and the two last lots on either side of the road that terminates at the intersection.

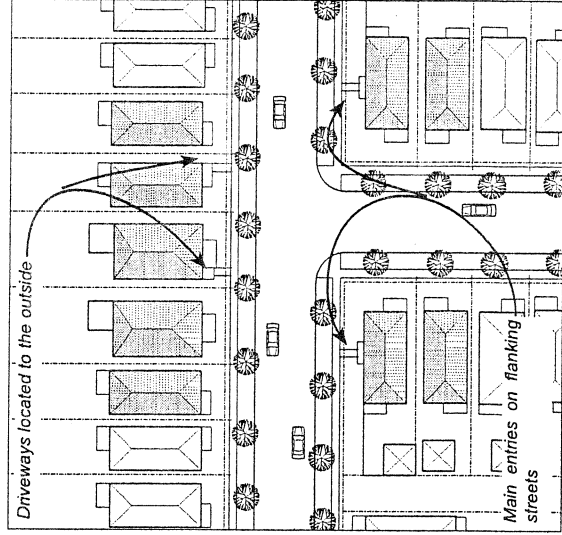
- special architectural features such as second storey porch, bay window, or gable elements, should be incorporated into the homes at the top of the 'T' intersection and homes should reinforce the community theme;
- the main entries of homes on the corner lots should be located on the flanking street and facing the homes at the top of the 'T' intersection;
- garages shall be located unobtrusively and recessed behind the front building face; and,
- garages should not be located directly at the top of the 'T' intersection.



A dwelling with a front porch open to the flankage street and fencing to reinforce the corner



Dwellings located at the intersection of two streets shall address both streets

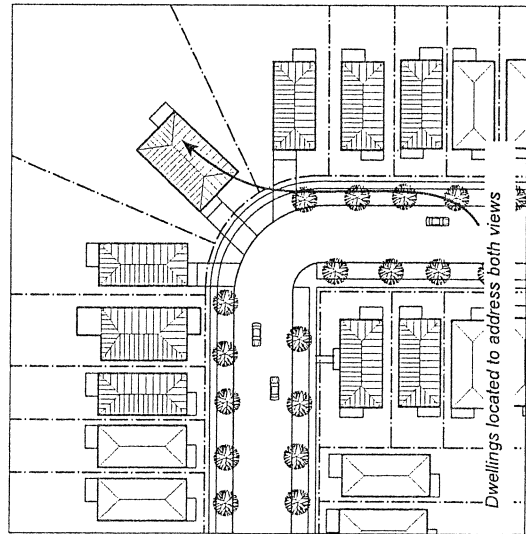


Dwellings located at the 'T' intersection should be considered as a group

4 Vista Termination

Principle: Homes at the curve of a street and on cul-de-sac should be considered as a group, and similar treatment should extend two or three units on either side of the curve.

- quality materials and architectural features shall be maintained on homes;
- where possible, garages shall be located unobtrusively and recessed behind the front building face; and,
- driveways shall not merge at the pavement edge of the street.



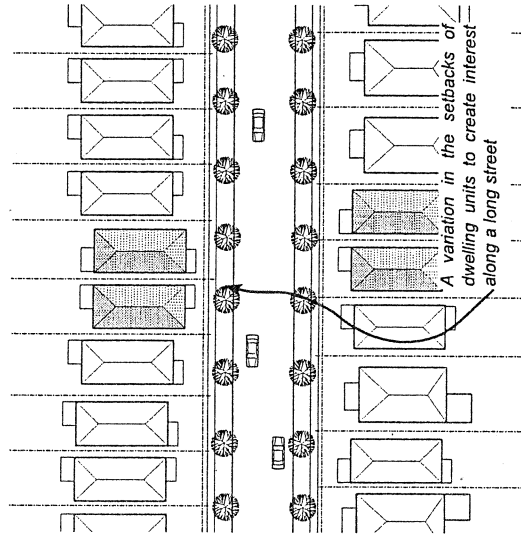
Considerations for vista termination to improve views along a street

5 Mid-Block Lots

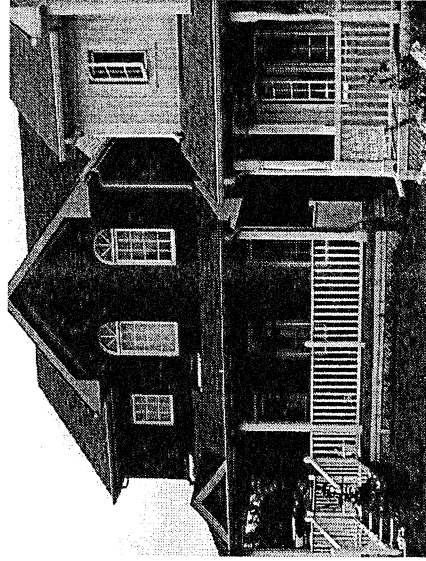
Principle: Create variation on a long street.

Where there are long streets, there are opportunities to introduce different setbacks or special features to create visual interest, where permitted by the Zoning By-law. Setbacks and features should be repeated on the opposite side of the street. These can be repeated at lesser divisions of an exceptionally long street.

- provide an alternative siting for one or two homes at mid-block along a long street; and,
- incorporate special architectural features mid-block to vary the overall impression of built form.



Mid-block conditions add variety



Varied setbacks create interest along a long street

6 Window Streets

Principle: Important windows into a community occur when streets parallel or cul-de-sac terminate at an arterial road. Homes facing the arterial should reflect the theme of the community.

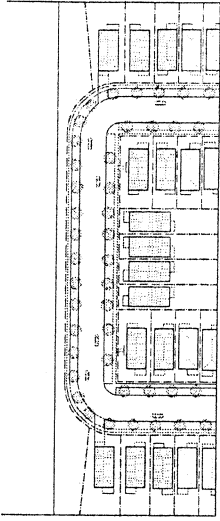
Passers-by gain an impression of the community from window streets. Window streets may be proposed as important component of the streetscape of Bathurst St. and component of the interface of residential and employment uses in the neighborhood.

- lots flanking a window street or on a cul-de-sac should be considered corner lots, i.e., front, exterior side, and rear facades of the homes should have consistent and quality materials and windows treatments;
- homes flanking a window street or on a cul-de-sac should have features such as porches, turrets, and bay windows on the corner facing towards the arterial road;
- the siting of bungalows on window streets should be discouraged;
- upgraded architectural detailing along window streets such as decorative facade elements or accent materials, shall be encouraged;
- safe pedestrian connection to arterial sidewalk shall be encouraged; and,
- dwellings should be sited such that garages should not dominate the view along streets visible from arterial window streets.

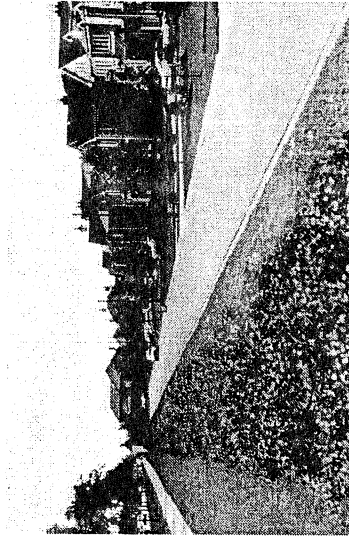
7 Lots Adjacent Open Space/Pedestrian Walkways

Principle: Homes adjacent active public spaces including open space, stormwater management facilities, greenway links, should address the open space.

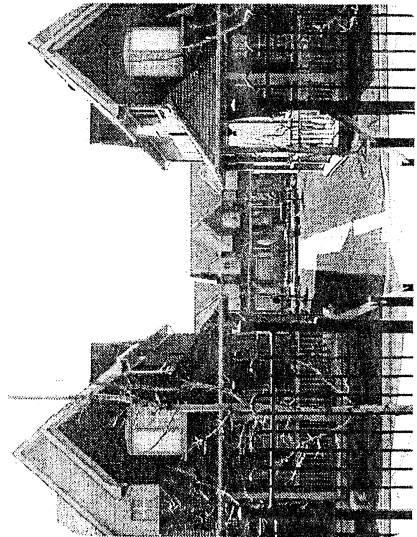
- architectural features such as porches, turrets, and bay windows should address the corner of the home adjacent open space;
- the type and quality of materials and window treatments should extend to the side of the home; and,
- garages should be located at the opposite side of the home.



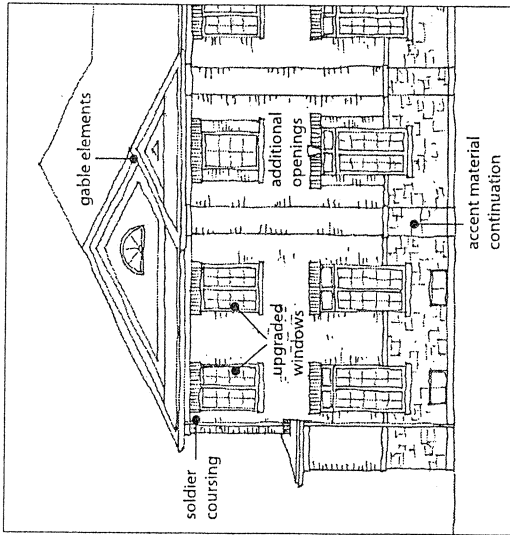
Units on window streets should be consistent in design



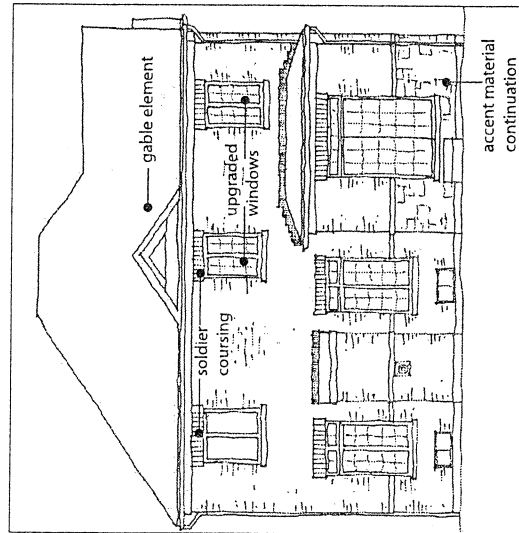
Connection to the arterial sidewalk should be provided



Wrap-around porches frame a pedestrian walkway



An example of an upgraded side/hankage elevation



An example of an upgraded rear elevation

8 Lots Adjacent Parks/Schools

Principle: Homes should be designed to frame adjacent public places and the streetscape should be designed to create the impression that consideration for a community place extends to the front door of the homes.

The following considerations should be given to the streetscape and homes adjacent parks and schools:

- homes across the street from parks should be designed to face the public space and create "eyes on the park";
- porches are encouraged to reinforce the objective of having eyes on the park and to provide a place from which to view activities;
- front, side, and rear elevations exposed to public spaces shall be highly articulated, with particular emphasis on the main entry design;
- homes should be a minimum of two storeys or be designed with two storey elements to create a strong edge to the area. Special consideration will be given to bungalows that are designed for this condition;
- garages should not dominate the view along streets adjacent public areas; and,
- where units back onto school sites, fencing should be permeable and the second storey of rear elevations of residences should be upgraded.

9 Lots Fronting Primary Streets

Principle: Homes fronting the major residential collector roads should be designed and sited to create a consistent street edge, and an active streetscape.

The following considerations should be given to the streetscape and homes fronting primary streets:

- front, side, and rear elevations exposed to public spaces shall be highly articulated, with particular emphasis on the main entry design;
- porches are encouraged to reinforce the objective of creating an active streetscape;
- dwellings should be sited such that a defined street edge is created; and,
- dwellings should be sited such that garages do not form a continuous visual domination along streets.

10 General Elevation Upgrades

Principle: When dwelling elevations are exposed to public viewing, the side and/or rear elevations shall be upgraded.

Lots sited in the following context shall require upgraded elevations:

- at gateway/ corner;
- at T-intersection/ vista termination;
- adjacent to school sites;
- adjacent to active public places, including open space, parks, stormwater management facilities, and pedestrian walkways;
- adjacent to roadways; and,
- on roads where severe grade differences expose elevations.

1.3 Setbacks

Homes are generally encouraged to be located close to the street to reinforce a strong street edge. However, variations within the setback shall be permitted within a specified range.

Front, side, and rear setbacks shall conform to municipal by-laws.

1.4 Housing Types

Medium density housing to include townhouses and semi-detached units should be encouraged to develop adjacent to Bathurst St. so transit use is facilitated on the arterial and some sound attention created at the neighborhood's edge.

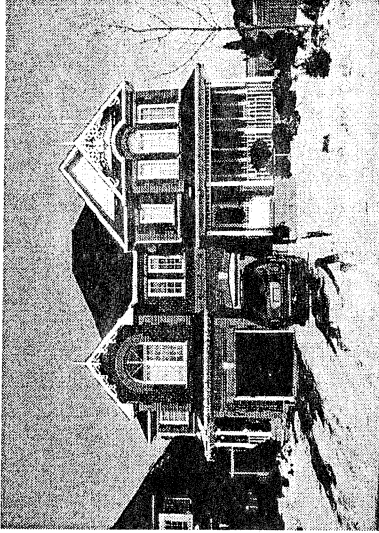
1.5 Architectural Styles and Influences

1 Single-Detached and Semi-Detached Elevations

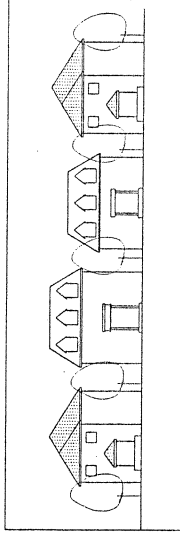
Principle: A variety of complementary designs should be offered to form an interesting residential streetscape. A minimum of two elevations should be offered for each type of unit.

It is not the intention of the guidelines to dictate that a specific style dominates throughout the neighborhood. Instead, architectural styles should be consistent and, where they include elements of historic detailing, those elements should not be mixed with other styles in the same building. Accordingly, the design of building elevations should be guided by the following principles:

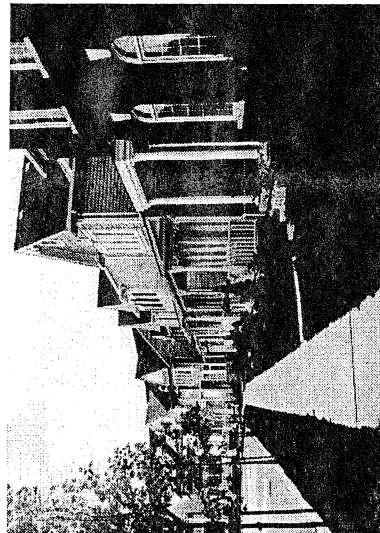
- there should be consistent application of architectural styles, materials, and colours within the community;
- architectural styles should be complementary;
- front doors should be visible from the street and should predominate the garage;
- front entries shall be considered important elements of the front elevation and should include architectural features such as roof articulation, porches, etc.;
- windows should predominate the front facade;
- window frames should be of aluminum, steel, or wood and should vary in design to distinguish individual units within a block;
- front facades and main entries of semi-detached homes should have similar architectural features and coordinated window placement;



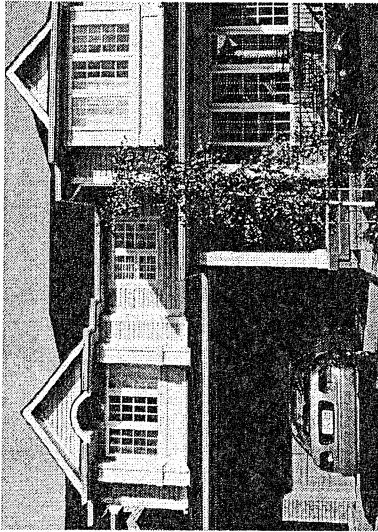
Semi-detached dwellings with a unified design



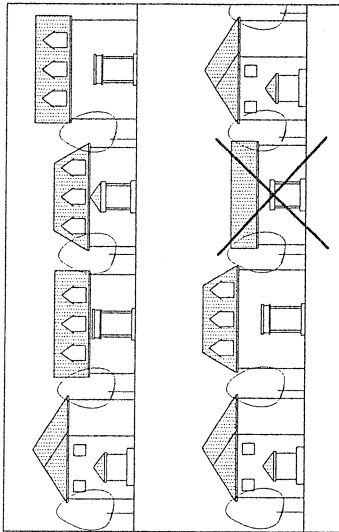
Varied elevations create a more interesting street



Siting units close to the front lot line creates an active streetscape



A strong front entry to the main dwelling unit



A 1 1/2 storey bungalow elevation is recommended to maintain an appropriate variation in height along the street

2. Townhouse Elevations

Principle: Townhouses lend variation to a residential streetscape.

The guidelines for single-detached and semi-detached elevations shall apply to townhouses. Additional guidelines pertaining to the design of townhouses are:

- the siting, massing, and facades of townhouses shall be coordinated on an individual and block basis;
- townhouse elevations shall vary to distinguish individual units; however,
- a consistent and complementary pattern of architectural features shall be applied to define a grouping of townhouses;
- rooflines shall vary to add visual interest and to differentiate a long group of units;
- townhouses adjacent single and semi-detached homes shall have architectural features to reduce the perceived scale of the building and to blend with adjacent homes; and,
- where townhouses face public parks and public facilities, a regular repetition of elevations should be considered to present a more formal streetscape.

3. Single-Storey Units

Principle: Single-storey homes add to the diversity of housing types and the residential streetscape.

- a minimum of two, single-storey units shall be sited together;
- single-storey units shall not be considered on corner lots;
- single-storey units shall have a 1 1/2 storey design to better integrate with two-storey adjacent residences;
- the rooflines of single-storey units shall be compatible with adjacent two-storey units with a minimum front elevation roof pitch of 8:12; and,
- front entries shall be emphasized with gables, dormers, and other roof and entry treatments.

4 Main Entrances

Principle: Front entries create an attractive and active streetscape.

- front entries shall be the most prominent aspect of the front elevation;
- front entries shall be distinguished through the use of framing materials, colour, and architectural forms such as porches, arches, and front steps;
- steps shall be designed as an integral component of the unit, in proportion to the overall dwelling; however,
- more than three steps at the front entry should be avoided, unless integral to the architectural design of the building such as appropriately detailed railing, integrating the steps into the design of the porch, designing the steps in concert with the landscape or providing landings; and,
- precast steps can be used for a flight of stairs no greater than three, more than three steps requires steps precast as a single unit or poured in place.

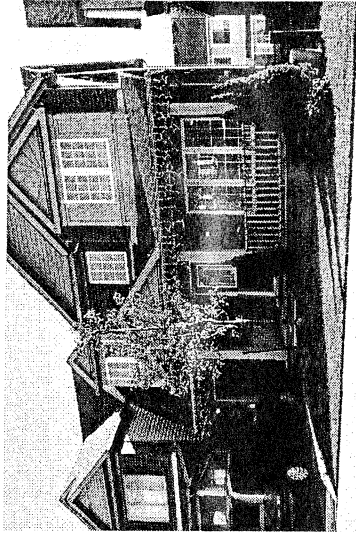
5 Porches and Verandas

Principle: As an extension of the house, porches and other entry features establish the necessary linkage between the public and private realm of the street.

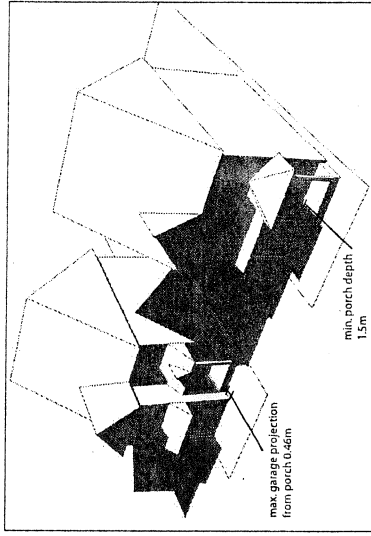
- a minimum of 50% of units shall be designed to incorporate a porch;
- the width of the porch shall encompass the door and windows of the front facade of the unit unless approved by the control architect;
- porches on single-detached units shall be deep enough to allow a seating area of a minimum depth of 1.5m;
- a maximum of 3.2m is allowed between the finished

porch level and the bottom of the soffit, and shall be constructed of materials used elsewhere on the home;

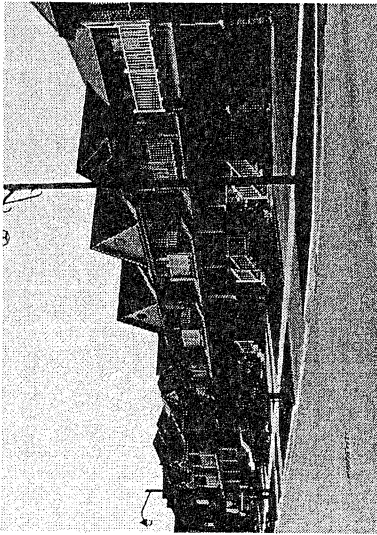
- a second level terrace or building over the porch is permissible; and,
- porch steps shall be detailed in the same material as the porch floor, with the exception that wooden steps are not permissible.



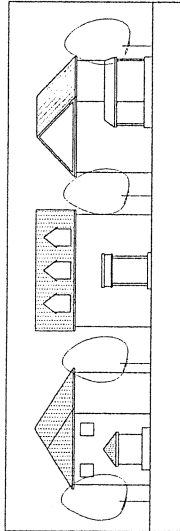
Residences are made attractive with the incorporation of a front porch



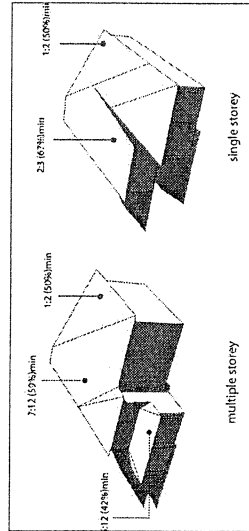
The garage is encouraged to be flush with or recessed from the main wall



Various rooflines create visual interest



A varied roofscape creates visual interest on the street



Recommended minimum roof pitches for dwelling units

6. Windows and Doors

Principle: The proper placement and coordination of windows and doors is essential to creating a pleasing facade and should sensibly relate the interior space to the street.

- windows shall occur on the front facade of all housing types;
- windows should predominate the front facade;
- where possible, a window, sidelights, or transom should accompany the front door;
- windows on all facades should vertically and horizontally align with each other and other features of the house;
- architectural details should be implemented to emphasize doors and windows; and
- sliding doors are not permitted on the front or flankage that are visible from the street elevations.

7. Roofs

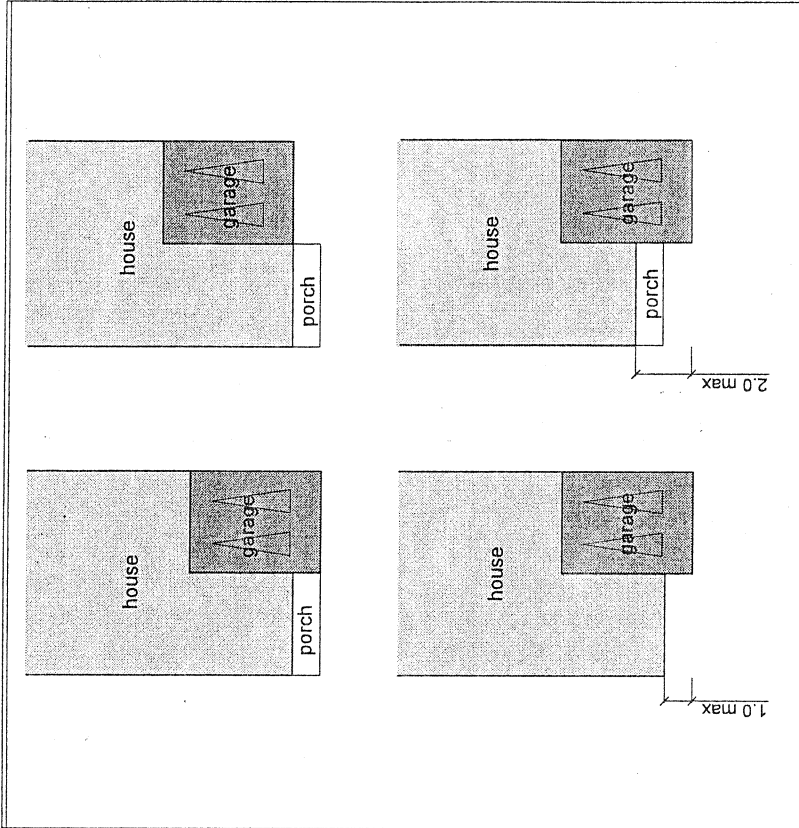
Principle: Variations in roof massing accentuate individual residences and contribute to an appealing streetscape.

- front facing roofs should include accent gables, dormers, and a variation of rooflines to lend variation and to reduce the pitch of the roof;
- to provide visual interest and variety, different roof slopes are allowed and encouraged, however, roofs should generally have a minimum pitch of 6:12. 7:12 is required for main roofs facing the street. A pitch of 5:12 is appropriate for secondary roofs such as porches and garage roofs. The front elevation roof pitch shall be a minimum of 8:12 on a single-storey unit;
- the soffit shall have a consistent minimum overhang of 305mm;
- all stacks, gas flues, and roof vents shall not be visible from the front or from side elevations in the case of corner lots;
- gas flues should be located near the roof ridge to reduce their height;
- all metal chimneys shall be boxed-in and finished with cladding;
- flat roofs are not allowed except on porches and side extensions of the dwelling unit; and,
- skylights shall be located on roofs not visible from the street and should have a flat profile.

1.6 Garages and Driveways

Principle: The design and material of attached garages should complement the main dwelling unit to create a cohesive streetscape.

- .1 Attached Front Yard Garages
 - attached garages must be a natural extension of the design, massing, and materials of the main dwelling;
 - garages shall not exceed the percentage of the width of the main dwelling allowed by the by-law;
 - maximum interior garage width should comply to the Town's Zoning By-law;
 - exterior parking pads and driveway width standards shall be in accordance with the Town's Zoning By-law;
 - where possible, garages should be recessed or flush with the habitable portion of the unit;
 - where this is not feasible, garages may project beyond the main front walls of the home in accordance with the Town's Zoning By-Law;
 - front entry features and other architectural elements should be placed close to the street like to reduce the visual dominance of the garage;
 - garages shall be setback from the property line in accordance with the By-law;
 - an opening of 5.5m or wider require two single-width bay doors;
 - lighting to identify garages and street numbers are required;



Garage options for the community. Flush and recessed garages are preferred, projecting garages shall accord to above dimensions

- a maximum of one step is permissible leading from the interior of the garage to the house;
- additional steps may be allowed with a deeper garage and/or if the steps are recessed to remove obstruction; and,
- garages shall complement the design of the main dwelling.

Builders are responsible for ensuring that all relevant provisions of the zoning by-law are met including minimum setbacks, building/terraces over the garage, and permitted driveways widths. Builders will be required to prepare plans for single and double car garages for detached dwellings to provide a variety of housing types.

2. Rear Yard Garages

Principle: Garages located in rear yards offer variety and reduce the number of garages located in the front yards.

Garages can be located in rear yards by means of a driveway running the depth of the lot to the rear yard or by means of a driveway from a flanking street on corner lots. Garages can be detached or attached to the dwelling.

- a rear yard garage is possible on lots with a minimum depth of 30m;
- a rear, single-car garage is possible with a 10.9m minimum lot width, a detached double-car garage with a minimum lot width of 12.2m, and an attached garage with a minimum lot width of 15.2m;
- garages shall complement the design of the main dwelling; and,
- the zoning by-law provides rear lot line setbacks and dimensions for rear yard garages.

3. Carports and Parking Pads

Principle: Where a rear garage exists, there is the opportunity to provide parking for a second vehicle under a carport or on a parking pad.

- the design of a carport shall reflect the design of the garage and main dwelling; and,
- parking pads shall be hard surfaced with, e.g., asphalt paving, poured concrete, or paving stones.

4. Driveways

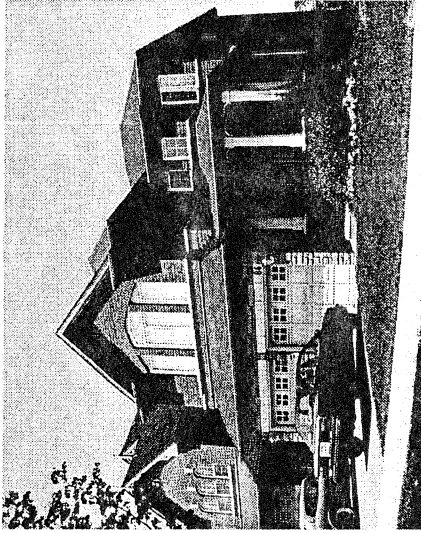
Principle: Driveways shall be unobtrusive.

- driveways shall generally be a maximum of 3m to meet a single garage or 5.5-6m to meet a double garage to maintain a pleasing streetscape;
- the maximum width and setback of a driveway shall be as per the Zoning by-law.

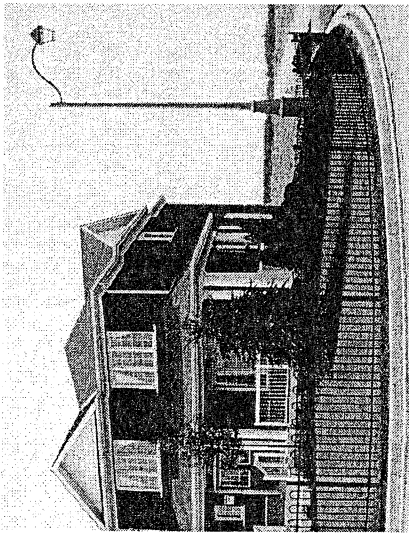
1.7 Exterior Colours and Materials

Principle: A pattern of materials types and colours should be used to create an attractive neighbourhood.

1. Foundations
 - stone, stucco, or brick should be used to clad foundation walls and should be detailed to emphasize the base of the dwelling unit;
 - concrete foundation walls should have a parged finish if it is not feasible to clad foundation walls in the materials listed above; however,
 - bare or unfinished concrete is not allowed; and,
 - where grade conditions apply, the wall material shall be stepped to permit a maximum of 900mm of exposed foundation wall.
2. Wall Materials
 - a variety of materials is encouraged, with brick and stone predominating. Calcite or concrete brick shall not be permitted;
 - the use of aluminum, high quality fibre cement siding (hardi-board), composite cement board, and stucco may be permitted. Vinyl siding is not preferred;
 - dwellings that are all or primarily stucco or vinyl cladding shall be limited to a maximum of 10% of all dwellings on a block face. This does not apply to bay windows, box windows or other feature elements;
 - units that are all or primarily stucco or vinyl cladding shall have strong architectural detailing, a visible masonry plinth, and shall incorporate architectural features such as porches as per brick and stone clad homes;
 - material changes are discouraged except to differentiate towers, bay windows, and other additions;
- additional materials may be used to draw attention to a particular feature or to break a tall or wide massing;
- detail materials and special masonry such as trims, stone sets, rock-face brick, keystones, etc. may be used around windows and doors; and,
- the material used for the front facade shall wrap to a minimum of 1200mm onto the side elevation of the dwelling unit; except,
- exposed flankage facades shall be of the same materials as the front facade.
3. Roof Materials
 - roof colours shall include a variety of hues including grey, black, and brown with some variation along the block; and,
 - the roof material of the house shall be the same for all secondary roofs such as the garage roof and a porch.
4. Exterior Colours
 - a consistent colour palette shall be used throughout the neighbourhood to create a cohesive identity;
 - various packages of complementary colours shall be developed;
 - a variety of colour packages should be offered to add visual interest;
 - adjacent units shall not be of the same colour package;
 - the same colour package may be repeated every fourth unit;
 - the entire streetscape of a block shall be considered and coordinated when determining the colour scheme for individual lots; and,



Stone cladding and a masonry band add variety to a predominantly brick home



Consolidated utilities contribute to a less cluttered streetscape

- different blocks of homes shall have different colour packages to promote the sense of distinct residential enclaves within a neighbourhood.

5. Garage Exterior Materials and Colours

The exterior colours and materials of garages shall be compatible with the main unit with particular attention to aforementioned priority lots.

1.8 Grading Conditions

Principle: Special consideration and architectural features shall be incorporated into the design of dwelling units on lots with sloped conditions.

Lots with a grading differential of more than 900mm measured from grade to the finished first floor shall be built to the following standards:

- wall cladding shall step to minimize an exposed foundation wall, to a maximum height of 900mm;
- an entry feature or porch should be added to the front entry;
- a flight of stairs shall have a maximum of six steps leading to an entry or porch. Where grade conditions mandate more steps, the design of the stairs will be reviewed on a lot by lot basis;
- window proportions shall be adjusted to create a balanced first and second floor elevation; and,
- the roof pitch over the garage should be steeper to reflect a greater vertical proportion.

1.9 Utility Service Metres and Mechanical Equipment

Principle: Utility locations are to be coordinated with the design of the units to reduce their visual impact on the streetscape.

- where possible, metres, metres, air conditioning units, vents for dryers, exhaust fans, furnaces, hot water tanks, etc. shall not be located on an elevation facing the street, and shall be located where they can be screened;
- where possible, utilities should be consolidated in one location and/or incorporated into the design of the unit to minimize visual clutter. Location of boxes should be determined at Draft Plan or Subdivision Design stage;
- utilities shall be recessed into the side elevation of the dwelling unit; or,
- utilities, excepting gas metres, shall be recessed under the floor of the porch or other architectural elements such as projecting low walls or niches to screen them from view; and,
- only where absolutely necessary may appropriate landscape and colour treatment be the sole means of screening utilities.

1.10 Utility Building

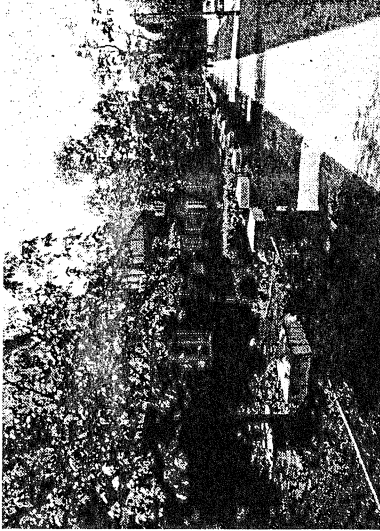
Principle: Buildings to accommodate utilities such as telecommunications or hydro, located within the community, should be designed to ensure appropriate design treatment.

- utility buildings should reflect residential design characteristics, including the use of pitched roofs, articulated facade design, and materials of the residential neighbourhood;
- the siting and design of utility buildings should be considered at a streetscape level;
- associated air condition units and mechanical equipment should be oriented away from adjacent residential areas, school buildings, and play areas;
- landscape treatment should be provided, without interrupting access for utility companies;
- utility buildings should be located to minimize public visibility, and;
- when they are located in an open space or SWM pond area, utility buildings should be treated as a feature with appropriate architectural treatment in terms of massing, decorative details, and materials.

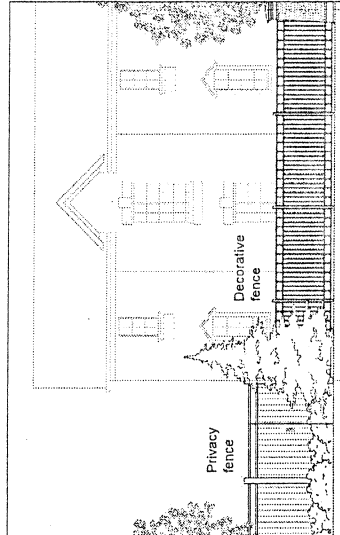
1.11 Fencing

Principle: Fencing creates privacy for residences and defines a street.

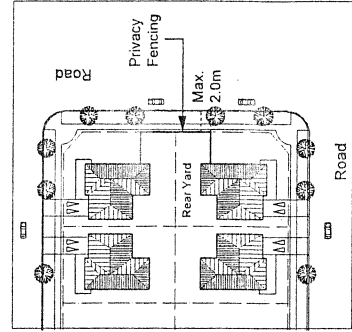
- 1 General Fencing
 - fences, hedges, and walls should be a maximum height of 1.8m as per the Town's Fence By-law to maintain visibility along a street;
 - a variety of fence styles and materials shall be repeated in the community; and,
 - wooden fences will be treated with exterior stain for longevity.
- Acoustic fences and walls employed as a means to provide noise attenuation for private homes and amenity areas shall comply with the following guidelines.
- acoustic fences shall provide adequate visual and physical buffering to the residences without creating an uninviting wall;
 - if masonry piers are used on acoustic fences they should be the same material and colour as all entry elements in the community;
 - acoustic fences should be designed to incorporate stylistic elements and/or materials complementary to the buildings, units, and architectural features in the community.
 - the acoustic fence should extend beyond the end of the house 1.5-2.0m; and,
 - where a noise attenuation fence exceeds 20m in length, the fence shall be articulated and/or incorporate variations.



An attractive fence is a precedent for residential fences along primary streets



A decorative fence (1.2m) complements a wooden privacy fence (1.8m) along a side yard



Privacy fence should include a fence gate on return

- privacy fencing should be designed to incorporate a gate on the portion of the fence that returns from the lot line to the side wall of the unit;
- where possible, a privacy fence should project from the dwelling at a recommended 1.5-2.0m distance beyond the end corner of the unit;
- where required, rear and side yard fences shall be consistent in design, colour, and materials with the front yard fence; and,
- fences provided by the developer/builder shall be subject to review by the control architect.

2.0 DESIGN REVIEW PROCESS

The design review process will coordinate the public and private realm architecture for the community, i.e., the site planning, streetscape, and residential architecture for the subdivision. The objective of the design review process is to assist the builder to comply with the conditions of the urban design guidelines and architectural controls. The review follows a prescribed and transparent process including the submission of drawings and materials, the preliminary response from the control architect, revisions as necessary, and sign off. This chapter outlines a six-step process:

- submission of drawings and materials for approval by the developer to the control architect;
- review of submission by the control architect;
- revisions as necessary;
- revisions to approved drawings;
- site review; and,
- implementation and monitoring.

Prior to submission of drawings for building permit, the architectural control process will be undertaken for the developer and/or builders.

2.1 Submission for Approvals

All design elements of buildings and their sitings must be reviewed and approved by the control architect and developer/builder engineer (where necessary) prior to the submission to the Town of Richmond Hill as application for a building permit.

In no case shall the control architect and the design architect be the same individual or firm.

2.2 Review Process

The design review process deals primarily with the exterior appearance of the units, the siting and arrangement of units within a block and on a specific lot. The following shall be submitted to the control architect for review:

- preliminary design proposals;
- final architectural drawings;
- site plans;
- sections, elevations, and illustrations of proposed streetscapes; and,
- samples of exterior materials and colours.

The builder shall submit drawings to the control architect in a form sufficient to evaluate compliance with the architectural controls. The materials presented for preliminary review need not be highly detailed but should be sufficiently representative for the assessment of design and site planning. Floor plans are provided for information and as a guide to evaluate the exterior treatment. Two sets of drawings will be submitted illustrating internal layout, entry conditions, elevations, fenestration, materials, and details. A preliminary design package typically includes:

- site plans;
- floor plans;
- exterior elevations and details;
- locations of special units/prime lots (if applicable);
- samples of exterior materials and colours; and,
- illustrations of typical streetscapes (if applicable).

This plan illustrates the areas within the development that require special architectural design controls. These are proposed for four particular locations within the community: gateway dwellings, neighbourhood junction intersection dwellings, vista termination, and t-intersection dwellings. Units in these areas have two levels of control depending on the visual prominence of their locations.

Level 1:

- a) Elevations: Architectural style and materials as in front elevation. Architectural detailing around windows and doors, base corbelling and sills, precast coining and lintels should be carried through to articulate the rear elevation. Massing should avoid a continuous flat wall.
- b) Roofs: articulation utilizing any of the following: accent gables, dormers and ridges, chimneys, etc...; minimum pitch 6:12.
- c) Windows: masonry detailing, variation of window sizes and/or other forms of window treatment.

Level 2:

- a) Elevations: materials as in front elevations, articulation to avoid a continuous flat wall
- b) Roofs: minimum pitch 6:12.
- c) Window masonry detailing, variation of window sizes.

Gateway Dwellings (G)

- a) Gateway dwellings should achieve Level 1 upgrade.
- b) The architecture should incorporate special built form at corner.
- c) Special landscaping is encouraged to create an entry condition.

Neighbourhood Junction Intersection Dwellings (N)

- a) Neighbourhood Junction Intersection dwellings should achieve Level 1 upgrade.
- b) The architecture should incorporate special built form at corner.

Vista Termination (V)

- a) Dwellings at vista termination should have elevations of a high design quality.
- b) The massing and grouping of dwellings should aim to create an interesting and unobtrusive view-line.

T-Intersection Dwelling (T)

- a) Architecture should have a highly articulate facade.
- b) Paring of sideyards is encouraged to form a landscaped area at the T-intersection.

